

public realm strategy

Supplementary Planning Document
for sustainability appraisal review

Rotherham Renaissance
...a new beginning

**Rotherham
town centre**

December 2008

draft

GILLESPIES



Rotherham Achieving



what is public realm?

Public Realm is the environment that surrounds us, it is the places in between buildings including streets, yards, alleys, arcades, squares and parks.

We use these places everyday to travel from A to B and to stop and watch the world go by. They are social places to meet friends or gather for celebrations. They are functional places to use to go shopping or park a car. They should be beautiful places and their quality can inspire and shape the character of a town.

The environment in towns and cities are strongly influenced by man made and natural features, creating scale, form and topography. Within the public realm the arrangement of trees and planting, paving, street furniture, signage, lighting and art can transform the environment and these tools create a palette for designers.

The best 'Public Spaces' lift the spirit, are easy to use, inspire activity and investment and above all are great places for people.



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Urdu

اگر آپ کو اس دستاویز کا خلاصہ کسی دوسری زبان اور / یا کسی متبادل صورت میں درکار ہو تو ہم سے رابطہ کریں۔

Chinese

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Farsi

در صورتیکه خلاصه ای از این مطلب را به زبان و یا شکل دیگری می خواهید لطفاً با ما تماس بگیرید

Arabic

اتصل بنا إذا تريد خلاصة من هذه الوثيقة بلغة أخرى أو بصيغة بديلة

French

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purpose of this document

The purpose of this document is to provide a clear strategy for the Town Centre's environment including a Vision, Framework and Design Guidance.

The Public Realm Strategy provides a tool-kit resource for Rotherham Metropolitan Borough Council (RMBC) to guide current and future public realm improvements within the Town Centre.

The strategy will assist in:

- Setting design and quality principles
- Establishing council policy for council led projects, private developer schemes and other public sector projects.
- Producing and guiding development briefs.
- Undertaking planning negotiations and setting quality expectations. The council will require developer contributions for public realm (capital and revenue costs). Refer to current council policy for further details.
- Attracting funding and coordinating with funding bodies.



how to use this document

The following table describes the proposals within this document and sets out the purpose of each section.

Public Realm Proposals:

Section 4 - Public Realm Vision

Four big themes create the **vision** :

- **Bring Harmony to the Town Centre:**
A public realm palette of materials, furniture and features, which can lift and tie together the environment
- **Reclaim the Riverside:**
A vision for the riverside environment.
- **Green Framework:**
A network of green spaces and connections.
- **Rotherham Lighting Experience:**
A new reason to visit the Town Centre.

Purpose:

The purpose of this section is to describe the strategy for each of the four themes.

The Public Realm Vision should be considered and applied to public realm projects throughout Rotherham Town Centre. Projects should therefore identify and demonstrate strategic tie in as part of the design and approvals process.

Section 5 - Public Realm Framework

Develops the vision to suggest ideas and guidance for the whole Town Centre:

- **Spatial Hierarchy**
Provides a map for future public realm investment
- **Zones of Influence**
Applies the Vision to each zone in the Town Centre and provides design principles and propositions.

The Public Realm Framework establishes a **Spatial Hierarchy**. This maps the quality and level of investment expected for future public realm projects.

The Strategy has divided the Town Centre into a number of **Zones of Influence**. Future projects should establish which zone applies and consider the principles and guidance provided.

Section 6 - Design Guidelines

Detailed design guidance for all elements of the public realm concluding in a public realm specification for the Town Centre

The Public Realm Specification Matrix should be used as a baseline for all Town Centre improvements.

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introduction

- Strategy Approach
- Strategy Area Context
- Current Town Centre Policy and Development

section 1

strategy approach

Approach

Rotherham is entering a new and exciting phase of its development. The catalyst for change has been Yorkshire Forward's Urban Renaissance Programme, the first phase of which culminated in the production of the Town Charter (which sets out a series of goals for the Town Centre) and Rotherham's Strategic Development Framework (SDF). The aspirations within the Charter and the SDF highlight the importance of public realm in achieving the wider aims. Goal Eight of the Charter states that *'we want the best in architecture, urban design and public spaces for Rotherham and will no longer accept the mediocre or the banal'*.

The Project Brief sets out the following vision and objectives for Rotherham's Public Realm Strategy:

"to bring about transformational change in the image and identity of Rotherham by realising the distinctiveness and value of existing environmental assets, creating a sense of place and prosperous identity, attracting and drawing together individual regeneration projects and programmes, and stimulating activity and vitality leading to an increased sense of safety and security".

Objectives

The objectives are to:

- Promote an holistic approach to design – visual, physical, sensory, symbolic, emotional and cultural.
- Develop the aspirations in the 'Strategic Development Framework' for 'New Streets and Spaces' .
- Promote accessibility (in particular encouraging/requiring use of disability assessments for public realm proposals).
- Create active frontages enclosing existing and proposed public realm places and connections.
- Integrate the river and canal into the Town Centre, including emphasis on riverside access (whilst designing to alleviate flood risk).
- Provide for a quality environment, that is:
 - distinctive and unique in its sense of place
 - vibrant, inspiring and stimulating
 - attractive, safe, and welcoming
 - valued, well cared for, and used both day and night.
- Encourage connections between people, places and spaces and establish stimulating and vibrant routes through the urban area.
- Enrich and extend the network of parks and green spaces in the town.
- Create a robust, long lasting, well-maintained and easily managed public realm.
- Ensure continuity and connectivity between all committed and emerging development projects and regeneration initiatives.
- Promote active travel and create attractive and user friendly links to all forms of public transport in order to address increasing levels of obesity
- Create green and shady areas to encourage people to use outdoor space for health and well-being

The consultant team were commissioned in March 2007 to produce the Rotherham Town Centre Public Realm Strategy. The team were led by Gillespies (Urban Design/Landscape Architecture) who partnered with David Potts Associates (Consultation Specialist) in order to provide this comprehensive approach.

Status

Initially the Council's intention was to adopt this document as an Interim Planning Statement (IPS), which would be taken into consideration when determining planning applications. However, following comments from the Government Office during the consultation stage about the limited planning weight of IPSs, it is now intended to adopt the Public Realm Strategy as a Supplementary Planning Document (SPD). The SPD would stem from saved UDP policy (ENV3.1) and appropriate policies in the Regional Spatial Strategy. To achieve this we require the Strategy to be subject to a Sustainability Appraisal (SA), which will be published as part of a public consultation exercise.

strategy area context

Local context

Rotherham lies in the county of South Yorkshire, and is situated approximately 6 miles north-east of Sheffield in the valley of the River Don. It is served by two motorways with a good infrastructure of bus and rail links into the town.

The strategy area encompasses Rotherham Town Centre including its conservation area.



Rotherham location plan.

Rotherham Town Centre has been on a journey and the current plans for its renaissance bring with it both opportunity and a major responsibility.

Medieval town

Trading Town, centred around the Parish Church.

Expansion

Rapid expansion in late 19th Century. Construction boom and steel production in the early 20th Century.

Stagnation

Relatively unchanged apart from 70's construction on periphery. Decline of heavy industry and retail (competition from out of town retailing).

Renaissance

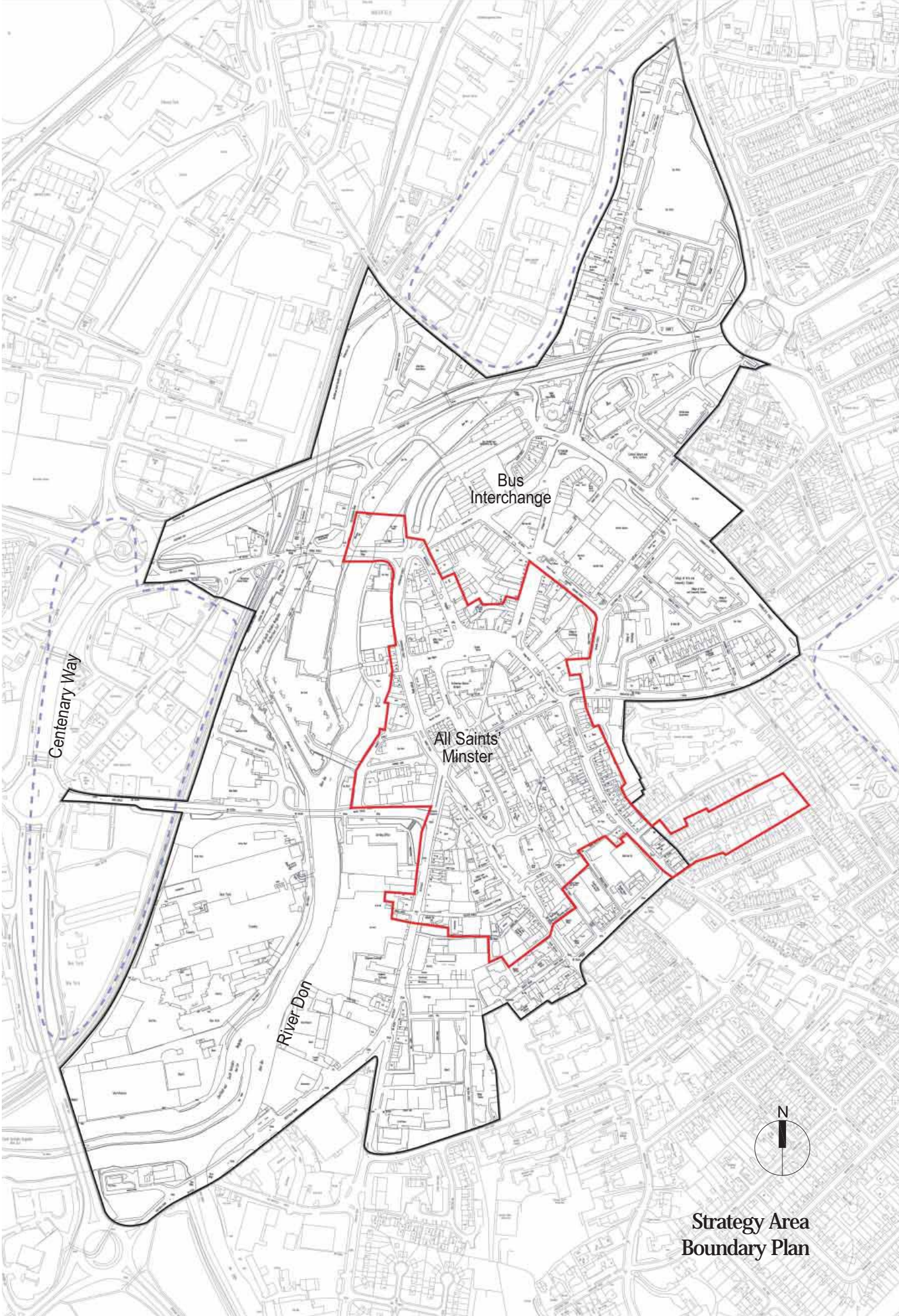
Explosion of new ideas and masterplans for radical, ambitious projects. Funding and developers are in place for the first wave.

Strategy Area Boundary

The strategy area boundary broadly includes Rotherham Town Centre and sets the scope and focus for this work.

Outside the strategy boundary a number of areas have been identified as having an important role to play and should be considered in future studies.

- Strategy Area
- Conservation Area
- - Key Areas Outside of Strategy Area



Strategy Area
Boundary Plan

current town centre policy and development

Policy context

Rotherham's ambition is illustrated by its comprehensive and aspirational plans for change, which have emerged over the last five years. The Rotherham Town Centre Public Realm Strategy must become a working tool for both existing areas of the Town Centre and areas of change and redevelopment, many of which are in the early stages.

This section of the report sets out how the Public Realm Strategy sits at the time of publishing, within the planning and wider strategic framework and the various studies that have been undertaken or are currently ongoing.

SDF

The Rotherham Strategic Development Framework (SDF) published in 2005 translates the ideas, dreams and aspirations expressed in the Town Charter into a physical structure for the future development of the town and identifies key projects that both individually and collectively will lead to the transformational change of Rotherham's urban centre. The Public Realm Strategy along with a number of other projects are a direct result of this work. In addition RMBC are undertaking a review and update of the SDF proposals.

Regional Spatial Strategy

National Government have worked with regional partners to develop, implement and monitor Regional Spatial Strategies, which set out Government's planning and transport policy for each region for a 15-20 year period. The strategies provide frameworks for determining planning applications, as well as for preparing local planning documents. The Yorkshire and Humber RSS is expected to be adopted in 2008.

Sustainable Community Strategy 2005-2011 (Updated 2008)

This key overarching borough-wide strategy describes the vision for Rotherham and key priorities and actions for Rotherham Partnership (the Local Strategic Partnership). The priorities are framed around five strategic themes; Achieving, Alive, Learning, Proud and Safe, and are underpinned by two cross-cutting themes; Fairness and Sustainable Development.

The Public Realm Strategy has strong links to all seven themes. For instance, the Achieving theme aspires to develop the Borough's identity, develop high quality spaces and buildings and improve external perceptions of Rotherham, all of which the Public Realm Strategy will have a clear impact upon. Similarly, the Safe theme expresses the need for safe, clean, green and well maintained neighbourhoods, with the Proud theme looking to strengthen local pride and improve external perceptions of Rotherham.

Local Development Framework

The Government has reformed the planning system to speed up the plan making process and be more transparent to members of the public. Along with all other Councils Rotherham Metropolitan Borough Council (RMBC) are preparing a new development plan for Rotherham to be known as the Local Development Framework (LDF). The LDF will be a portfolio of local development documents and will guide planning applications.

How does the public realm strategy fit in?

This strategy is intended to be adopted as a Supplementary Planning Document (SPD) by RMBC and provides a tool-kit resource to guide current and future public realm improvements, within the Town Centre.

This document has considered existing Town Centre policy and proposals and has been developed in coordination with emerging work:

- Rotherham Strategic Development Framework (SDF) and Rotherham Renaissance Charter
- Regional Spatial Strategy (RSS)
- Emerging Local Development Framework (LDF)
- Local Transport Plan (2006-2011)
- Rotherham Cycle Strategy
- Design Code for the Rotherham Town Centre River Corridor
- Guest and Chrimes Stage C report
- Shopfront Design Guide (July 2006)
- Express Parks - Development Proposals
- River Corridor and Flood Alleviation Scheme
- Conservation Area Appraisal and Management Plan
- Integrated Transport Study
- Railway Station Proposals
- Westgate Demonstrator Project

In addition to the public and a number of RMBC internal departments, the following groups have been highlighted as key consultees in the preparation of this document: Rotherham Town Team, Town Centre Strategy Group, Town Centre Planning Group, Rotherham NHS organisations and the Youth Council amongst others. Section 3 of this document sets out the approach to public consultation during the preparation of this strategy.



Rotherham Town Centre development sites

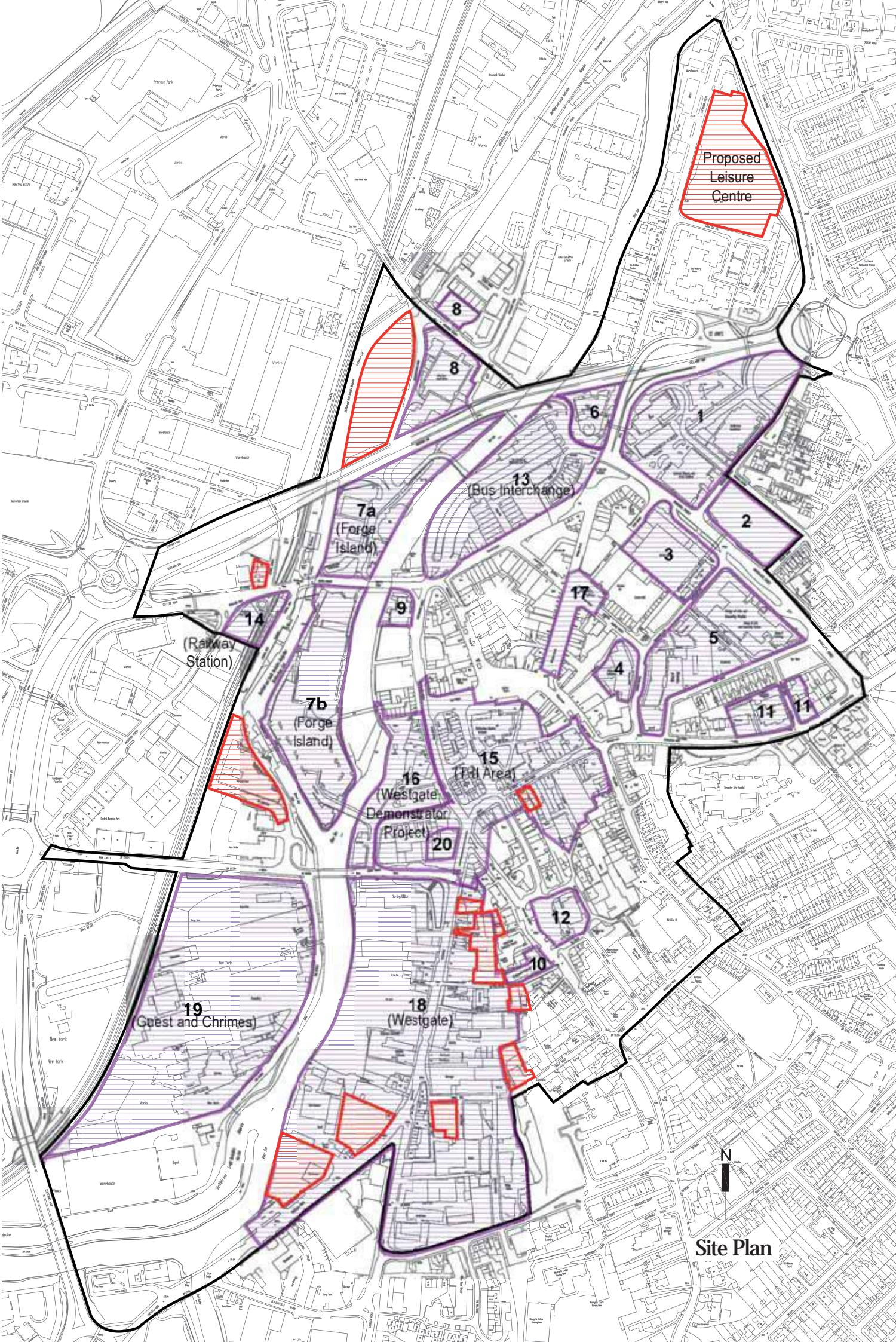
Current Development Initiatives

As described previously, Rotherham is on the cusp of major change. Before an approach to public realm could be produced, it was essential to gain an understanding and provide an overview of how all the development pieces fit together. The following pages gather all currently available information in order to describe the story as it stands in early 2008 and are not intended to pass comment on the proposals. It is important to note that each proposal is at a different stage of development, ranging from ideas through to firm proposals with planning permission. For the purposes of this strategy, the following information therefore represents the foundations on which the public realm proposals outlined later in the document have been built.

Three packages or 'lots' of development opportunities were put through OJEU process to seek development partners. Lot 1, specified the provision of a new civic accommodation building. Lot 2, specified the provision of a new cultural centre. Lot 3, specified the redevelopment of the sites which would be vacated as a result of the delivery of Lots 1 and 2. Delivery of each site is dependent upon a chain of events and completion is estimated for 2015. In addition a number of other projects including the Westgate Demonstrator and the Townscape Heritage Initiative are running parallel to this.

The Site Plan shown here and Development Site Matrix overleaf, aims to tell how the pieces fit together, their status and estimated timescale at the time of publication.

-  Ongoing Planning Permission
-  Development Sites



Development Matrix

Rotherham Town Centre development sites					
SITES	existing use	proposed use	status/issues	key players/ownership	documents/guidance
Site 1	Norfolk House Council Offices, Civic Buildings, Central Library and Arts Centre	Retail or other Town Centre uses (1.16ha)	Awaiting relocation of Council Buildings to Site 19	Council (Landowner), Express Parks (Developer)	
Site 2	Car Park	Town centre mixed use	Draft design submitted	Council (Landowner), Express Parks (Developer)	
Site 3	Outdoor Covered Market	Under consideration		Council (Landowner) Express Parks (Developer)	
Site 4	Rotherham College of Arts and Technology	College to be redeveloped on existing site	Proposals under development	Tech College	
Site 5	Rotherham College of Arts and Technology	Planned redevelopment of campus	Proposals under development	Tech College	
Site 6	Crinoline House Council Offices	Expansion of Bus Interchange/multi-storey car park	Awaiting relocation of council buildings to Site 19 Proposals dependent on plans for Site 13	Council (Landowner), Express Parks (Developer)	
Site 7a (Upper) Forge Island	Enterprise House Council Offices, Club, Car Park, Industrial Use			Council/Private	
Site 7b (Lower) Forge Island	Food Supermarket & Car park	Cultural Centre - Theatre, Museum, Library, Cafes, Residential (170 units)	Possible relocation of supermarket	Council, Express Parks (Developer)	
Site 8 Bailey House and Car Park	Offices	Under consideration	Awaiting relocation of Council Buildings to Site 19	Council (Landowner), Express Parks (Developer)	
Site 9	Car Park	Under consideration		Express Parks (Developer)	
Site 10 Eric Manns Building	Council Offices	Under consideration		Express Parks (Developer)	
Site 11	Technical College, Civic Theatre Clifton Court	Under consideration			
Site 12 Town Hall	Council Offices	Under consideration			
Site 13 Bus Interchange	Bus Interchange & Multi Storey Car Park	Possible expansion of bus interchange and rebuild of multi-storey car park and possibly a hotel	Awaiting decision on Site 6	SYLTE (Operator), Norseman Holdings (Owner), Hearts Head/Express Parks (Developer)	
Site 14 Railway Station	Rotherham Central Station	Refurbishment	Design being worked up	SYLTE/YF/RMBC	
Site 15 Townscape Heritage Initiative (THI)	Retail/Commercial/All Saints' Minster	Repair and restoration of historic buildings, Public Realm Improvements in Minster Yard and High Street, Living Over the Shops (LOTS)	Funding approved from Heritage Lottery Fund, Yorkshire Forward and RMBC	Council/property owners/English Heritage/HLF/YF	THI Sketch Design Proposals
Site 16 Westgate Demonstrator Project	Retail/Commercial/Residential	Riverside Apartments, offices, cafes/restaurants and high profile public space 'deck of cards'.	First phase of redevelopment under way	Council / Iliad development company/YF	Westgate Demonstrator Project Report
Site 17 Effingham Street	Square	Under consideration		Council	
Site 18 Westgate	Retail/Commercial	Proposed land use mix to be confirmed		Council / private sector	Design Code (Town Centre River Corridor)
Site 19 Guest and Chrimes	Site undergoing reclamation	Council Offices and mixed use development		Council and Evans Regeneration Investments Ltd	Guest & Chrimes Stage C Report (agreed)
Site 20 Westgate Chambers	Retail/Commercial	Mixed use			
River Don Corridor		Flood Alleviation Scheme	Detailed design under way for Town Centre corridor	Council, landowners	

physical analysis

- Built Form & Topography
- Movement, Links and Gateways
- Visual Framework
- Green Framework
- Flood Alleviation
- Public Realm Palette
- Issues and Opportunities
- What is 'Rotherhamness'?

section 2

built form & topography

Historic Urban Grain

Rotherham developed as an Anglo-Saxon town, and during the medieval period was the market centre for a wide rural hinterland, a role that was reinforced by its location as a key crossing point of the River Don. Until the mid-nineteenth century the town clustered around the Minster, with commercial life centred on High Street, College Street, Bridgegate and Church Street. A network of tighter grained medieval ginnels, lanes and yards enrich the character in the historic core including Vicarage Lane, College Lane, Snail Hill and The Red Lion Yard.

Rotherham expanded rapidly in the late nineteenth and early twentieth centuries, mostly due to the construction boom that coincided with the peak of Rotherham/Sheffield steel production period. Since the outbreak of the second world war the majority of changes to the strategy area have occurred to the north, where demolition of some terraced housing and the addition of the A630 road hierarchy can be seen.



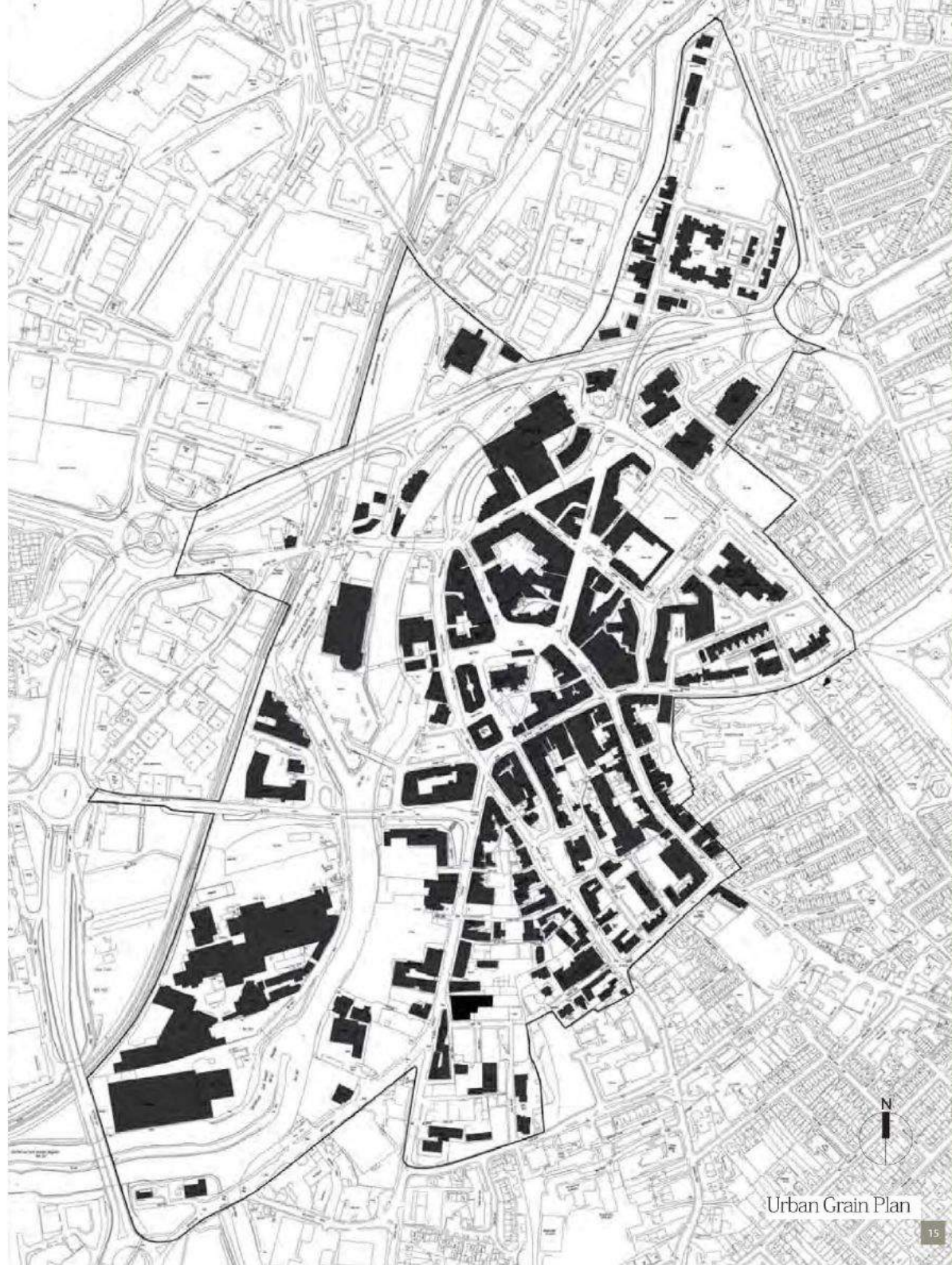
Late nineteenth century

Today's Urban Grain

This plan highlights how the layout of the streets mostly follows the original historic pattern.

Although the majority of medieval streets have been widened and new streets created, the central area of the Town Centre retains a medieval, organic character.

The grid like pattern of terraced housing to the north breaks down to accommodate the A630 (Centenary Way).

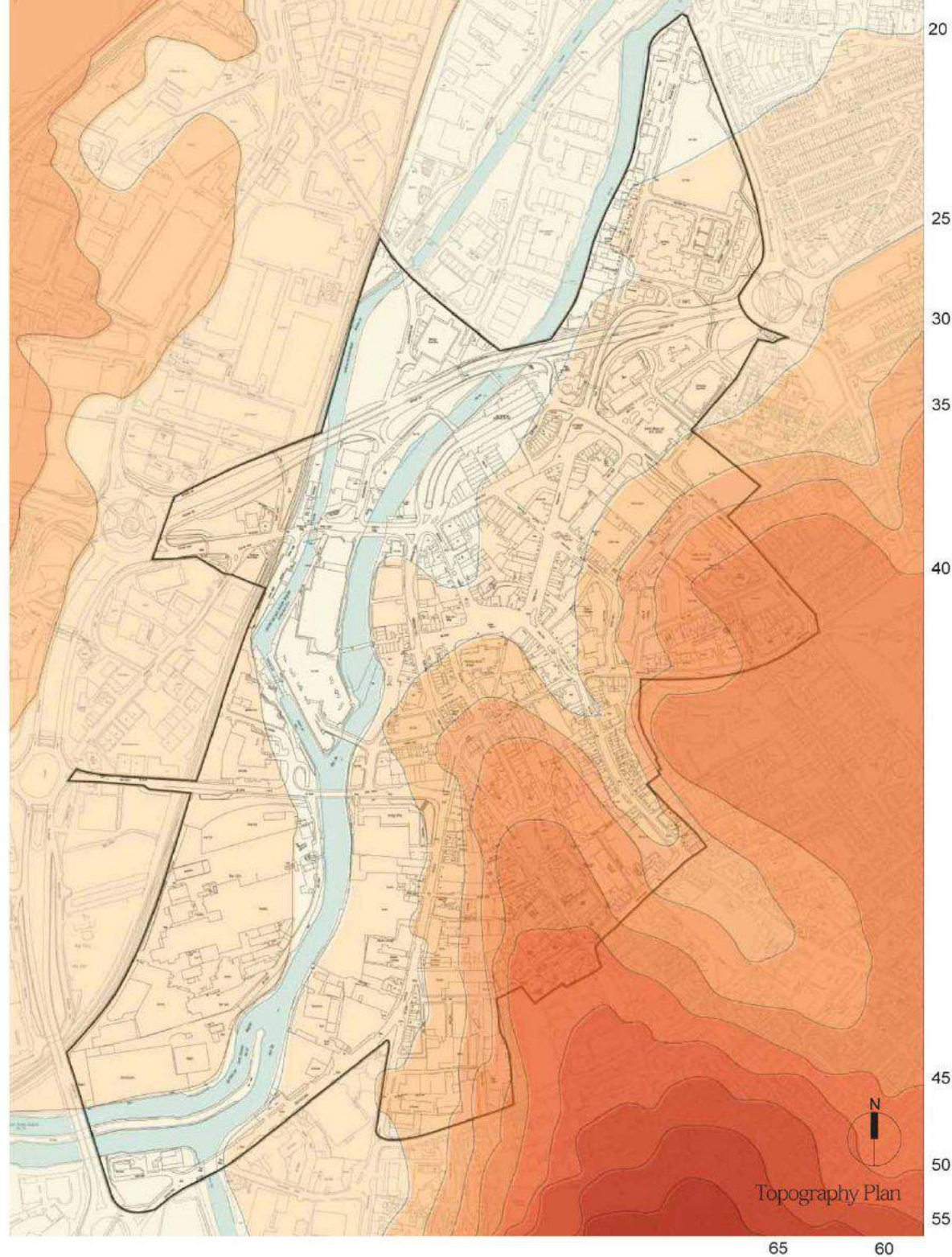


Topography

25 metre level changes within the strategy boundary give rise to some dramatic views into and out of the Town Centre.



Significant level changes allow views towards countryside



movement, links & gateways

Movement, Links & Public Space

This diagram clearly highlights the strongly defined pedestrian core within Rotherham Town Centre.

The western and northern edge of the town is dominated by the major vehicular route Centenary Way. The river and dominant 70's infrastructure create significant barriers to pedestrian movement.

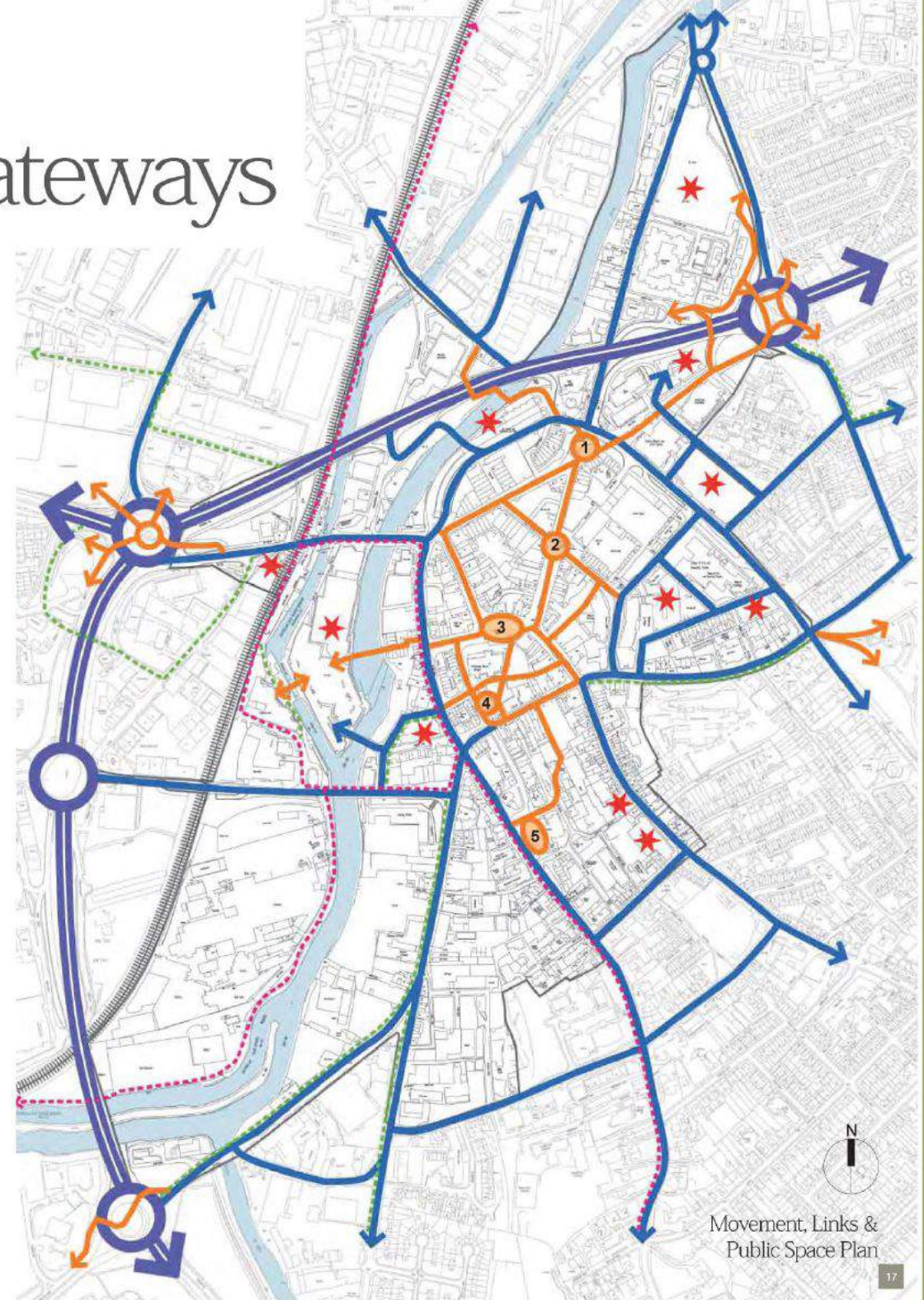
Five major public spaces have been identified which are predominantly located within the retail core:

1. Effingham Square
2. The Market Square
3. All Saints' Square
4. All Saint's Yard
5. Town Hall Square



Pedestrian movement is obstructed along Bridgegate. Its function as a link space to All Saints' Square is cluttered and confused.

- Centenary Way
- Vehicular/pedestrian routes
- Pedestrian routes
- Pedestrian generator
- Public Space
- National Cycle Network (Combined)
- Cycle Routes



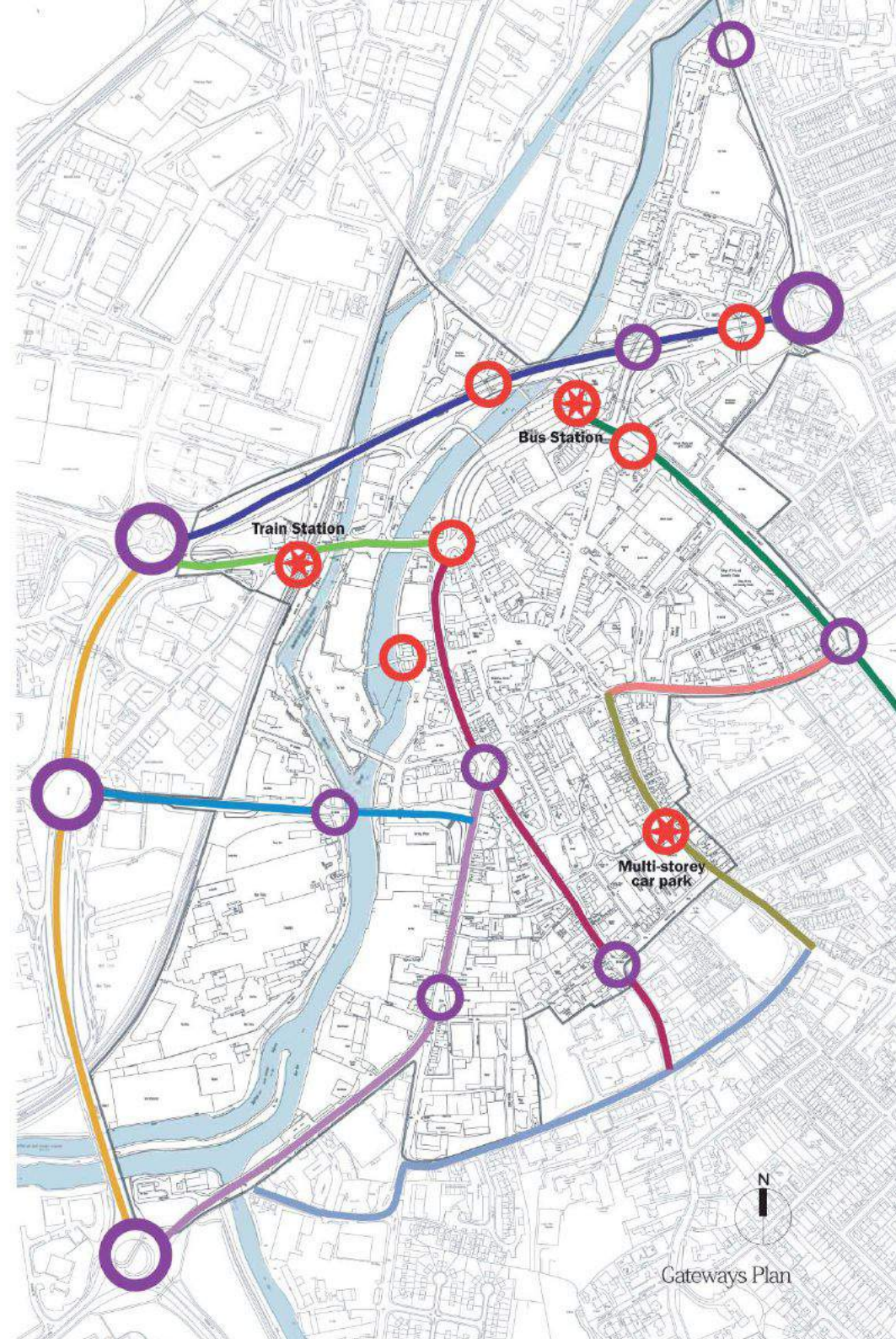
Gateways

Four main types of gateways have been identified within the Town Centre:

1. Vehicular dominant gateways, which are generally busy roundabouts or junctions, often incorporating pedestrian underpasses.
2. Smaller scale pedestrian gateways, which include underpasses, pedestrian bridges and arrival points.
3. Pedestrian generator gateways, such as car parks and public transport.
4. Linear gateways, such as roads (as identified through the Housing Market Renewal Programme).



Roundabout - important arrival point into Rotherham town



Gateways Plan

Pedestrian Riverside/Canal Access

The River Don and Canal needs to be reclaimed rather than hidden away.

This plan illustrates that large sections of the riverside currently have no pedestrian access.

Where access does exist the pedestrian environment is often poor with backs of buildings facing onto the river. In tandem with the Flood Alleviation Scheme and the significant opportunities presented by the areas of future change (shown in purple) a continuous riverside walk should be achieved.

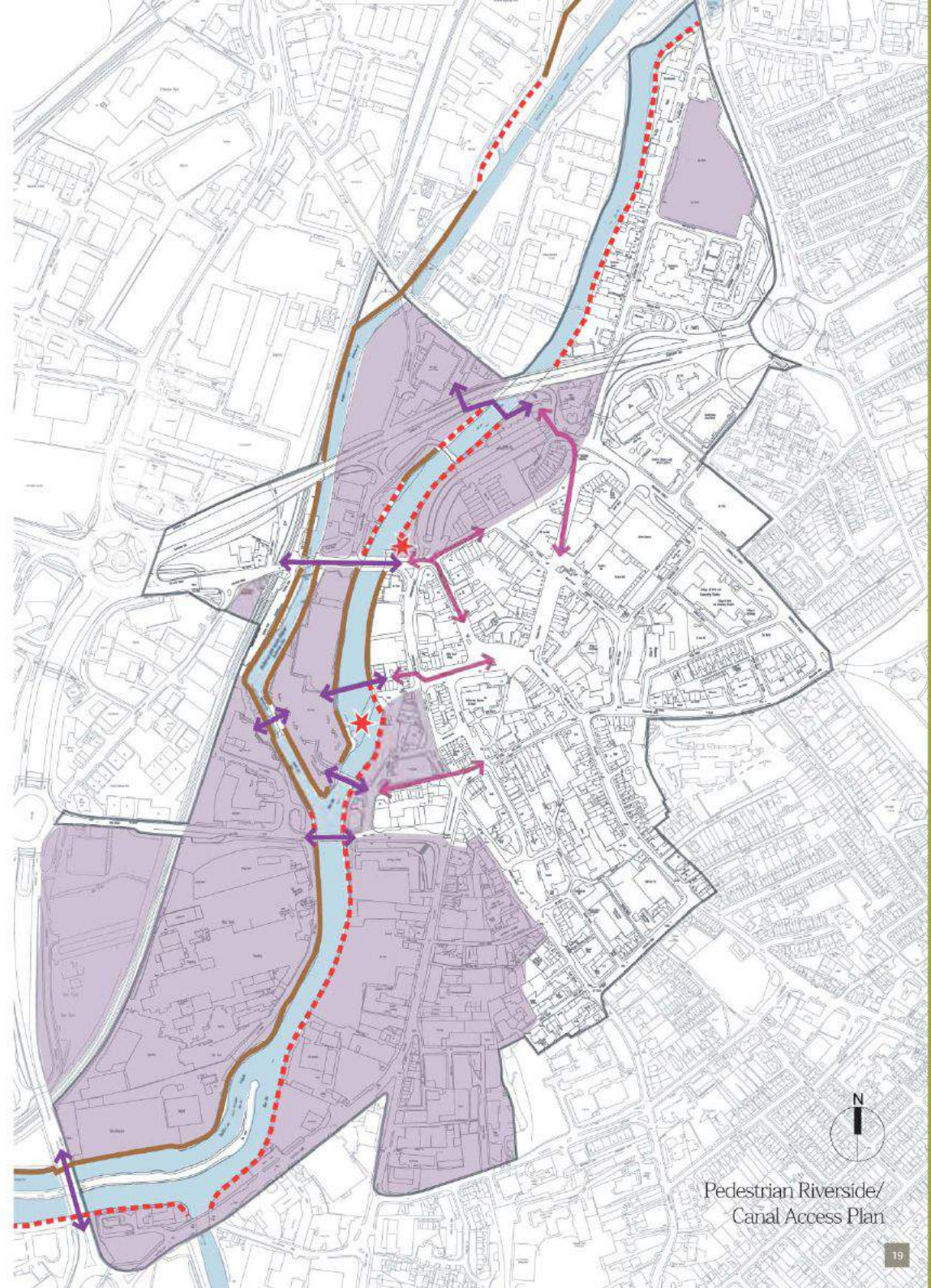
Currently links from the Town Centre to the river are lacking and the river should become more integrated creating a joined up network of routes.

In addition a number of existing and new destinations and icons should be arranged along the river. These include Our Lady's Chapel and the Weir, which should both be emphasised by future public realm treatment. The 'deck of cards' public space proposed as part of Westgate Demonstrator Project should become a major new destination within close proximity of the riverside. Furthermore, the opportunity to create a destination point in conjunction with the proposed redevelopment of the Guest & Chrimes site should be promoted.



Pedestrian riverwalks often stop abruptly. This barrier is in place for pedestrian safety adjacent to the bus interchange, however such layouts are typical of the challenges to overcome in terms of delivering a continuous riverwalk.

-  Existing Pedestrian Bridge Crossings
-  Existing Riverside Access
-  No Riverside Access
-  River Don and Canal corridor
-  Future Areas of Change
-  River Icons
-  Pedestrian links to river



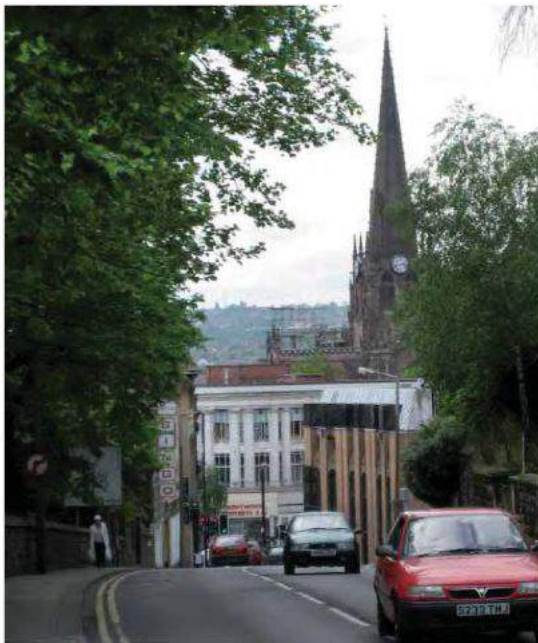
Pedestrian Riverside/
Canal Access Plan

visual framework

Vistas

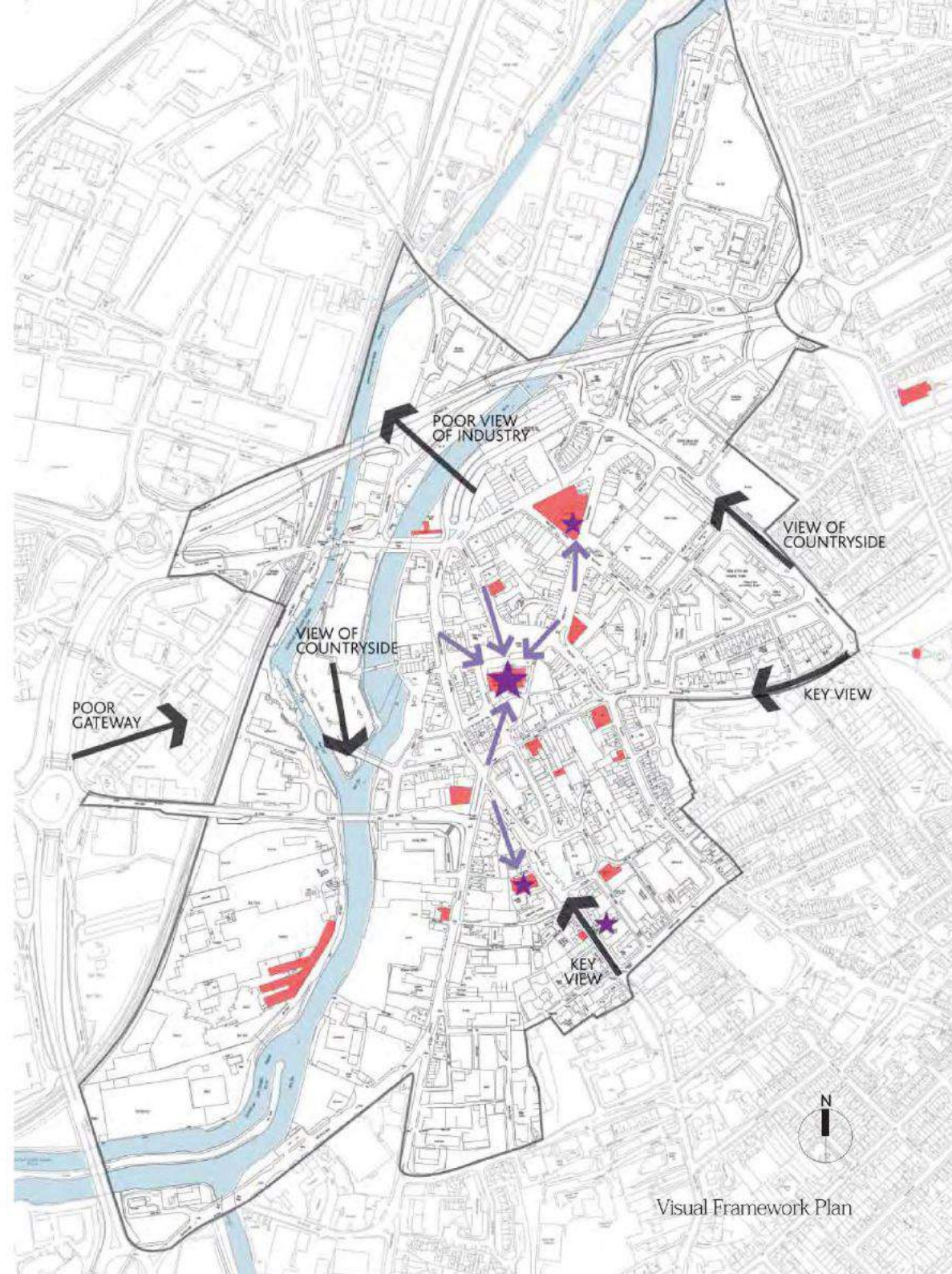
All Saints' Minster is situated as the centrepiece of the Town and becomes a frame of reference from all locations. In addition the spires of Talbot Lane Methodist Church and the adjacent church on Moorgate create an attractive skyline. The streets running towards the Minster create framed views and glimpses of it before opening up into All Saints' Square.

The topography is more dramatic than it initially seems. Panoramic and framed views of the surrounding countryside are particularly attractive. However the Town Centre periphery is dominated by infrastructure and industry. The River Don is largely hidden out of view and is a lost opportunity.



Attractive view of Minster looking west down Doncaster Gate.

-  Key Views
-  Important Views
-  Attractive Buildings
-  Listed Buildings



Visual Framework Plan

green framework

Green Assets

Rotherham Town Centre lacks green space, however Clifton Park and the newly proposed Wetland (part of Flood Alleviation Scheme Proposals) are both within walking distance. Within the strategy area only pockets of greenspace are evident, with the Minster Yard located in the heart of the Town Centre. In contrast the Town Centre does benefit from mature street tree planting within the pedestrian core (Effingham Street, Frederick Street and Upper Millgate) and landscaping at major roundabouts/vehicular gateways. However Rotherham Town Centre lacks a strong network of green corridors, which could tie together existing green assets.

Clifton Park

Clifton Park is Rotherham's premier urban park, five minutes walk from the Town Centre and occupying a 56 acre site surrounding Clifton Park Museum. Highlights of the park include the children's playground area, the Dell, the Bandstand, the Memorial Garden, the Big Beech Tree, and the Museum. A funding bid has been submitted to the Heritage Lottery Fund for the restoration of areas of historic landscape significance and to provide visitor facilities and water play areas. The park is of huge amenity value as a green lung for the Town Centre but needs an emphasised connection.

Wetland Area

As part of the flood alleviation and water management improvements being constructed (completion 2008) along the River Don, water flows will be instigated at Templeborough during a storm event helping to protect the local infrastructure from potential flood damage. The area set aside for this is adjacent to the Bradmarsh Business Park and will make a wetlands area attracting wildfowl and acting as a green and tranquil space for local people to enjoy, linking into the Town Centre Green Framework.

River Corridor

The River Don and Canal corridor features a number of green pockets and vegetation. In tandem with proposed flood alleviation improvements and the aspirations for a continuous riverwalk, a green corridor could be delivered. The achievement of this should be a requirement for emerging development proposals.

- Green Space/Landscape Areas
- Street Trees
- River Don & Canal



Green Space plan

flood alleviation

Water management & flood alleviation

Rotherham Council and partners (Environment Agency and Yorkshire Forward) have been studying the overwhelming influence the river has on the Town Centre, the upstream river environment and the potential flooding problems that can occur in the Town Centre.

The plan opposite shows phase 1 of the overall flood alleviation scheme. This phase is due for completion in 2008 and will provide new flood defences and a flood mitigation area to underpin the renaissance aspirations of opening up access to the riverside. The scheme will protect major road and rail arteries into and out of Rotherham in addition to existing businesses and proposed commercial development. The overall aim is a community wide scheme that will provide a full 1 in 100 year standard flood defence with the flood mitigation area becoming a community focused wetland managed by the Sheffield Rotherham Wildlife Trust.

Phase 2, a further 2km downstream through the Town Centre is currently in design stages with the aim of co-ordinating a comprehensive approach to flood control and water levels through the Town Centre.

Flood alleviation improvements through a community wide scheme need to be provided in Rotherham Town Centre in a coordinated way alongside riverside development sites to both reduce flood risk and ensure a coherent approach to riverside public realm and public accessibility. These together offer a major opportunity to improve links to and access along this untapped asset.

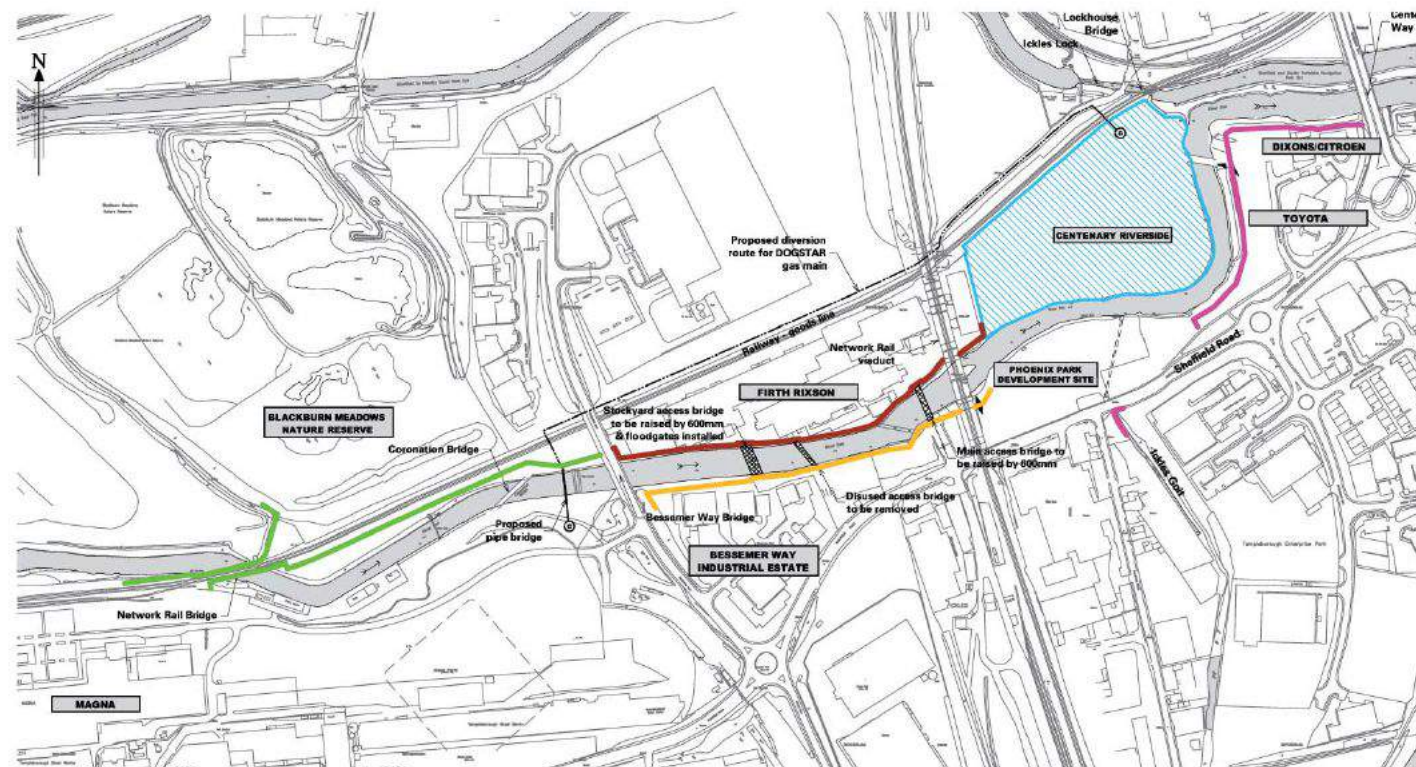


Illustration showing Phase 1 improvements (Wetland proposals).

Flood alleviation scheme

Key points regarding the flood alleviation scheme are:

- When complete it will provide a 1 in 100 year standard of flood protection and will be an overall community wide scheme that extends from Magna to St Anne's Road. The scheme is designed in a manner that defences can be raised in the future to allow for the impacts of climate change.
- Riverside access is being incorporated into the scheme wherever practical.
- Phase 2 of this integrated scheme will be implemented in a number of stages.

June 2007 flood

There are instances of historical flooding in Rotherham, most recently in June 2007. Significant areas of Rotherham Town Centre lie within Flood Zone 3 and guidance suggests that Climate Change will worsen likely flood risk. It is therefore important that we plan now for flooding, to minimise its likely environmental, economic and social impacts in the future. It is important in flood risk areas, that buildings are designed to be initially resistant, and then resilient to flooding. It is important to recognise that some of these techniques may impact on the external appearance of buildings, including their overall height.

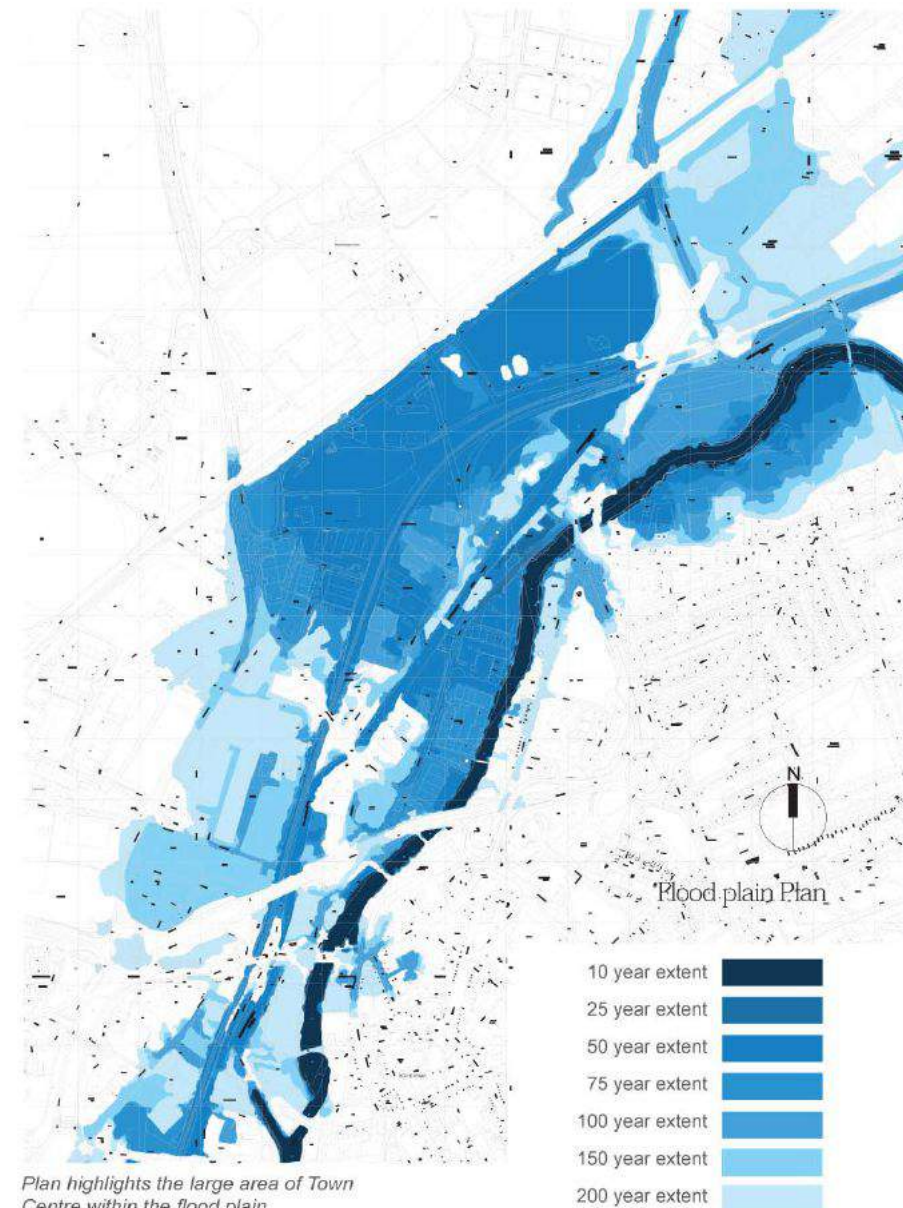
The provision of areas of public open space such as parks, play areas or public squares, in the areas most likely to flood will help to minimise the numbers of homes and businesses affected during flooding, both by storing flood waters safely and reducing the amount of new development taking place in the highest risk areas. Sympathetically designed streets, parks and other urban infrastructure, can have a significant cumulative impact on reducing instances of flooding. By incorporating green areas, water can be safely stored, evapo-transpiration can be increased and rainwater can soak away slowly into the ground. These techniques slow the movement of water into watercourses and sewers, where flooding can more readily occur. It is recommended that the use of permeable pavements on parking and hard-standing areas, the provision of permeable and planted areas throughout the town centre, and the designation of areas of open space in the highest flood risk areas should be considered.



June 2007 Flood - Photos illustrate the level the water reached, damage caused and the debris left behind.



June 2007 Flood - Illustrating partly submerged Tesco premises, Forge Island.



public realm palette

Surface Materials

The wide variety of public realm treatments within Rotherham Town Centre are the product of its evolution through various schemes and trends over the years. There are successes, however in general the public realm is disjointed and confusing, with too many different materials used.

Paving schemes from the last twenty years include the use of light brown, red and dark and light grey setts (concrete and clay) laid out in various patterns and details. These materials are generally dated, over complicated in places and in poor condition detracting from the quality of the buildings. In some locations as many as six materials are used together especially where various treatments abut.

A recent palette used in the Town Centre includes buff concrete aggregate slabs with 'tegula' concrete sett accents and concrete kerbs. This treatment is tidy, easily maintained and cost effective. The warm tones of the buff slabs and pink tinged colours of the 'tegula' compliment the colours of the Rotherham Red (sandstone) buildings. This has been upgraded to good effect in higher profile spaces such as All Saints' Square with the use of natural yorkstone.

The reuse of historic materials such as yorkstone and granite setts as seen in parts of the historic core is highly appropriate, attractive and adds a richness to the streetscape.

The issues raised above are not unique to Rotherham as town centres undergo continuous evolution. However a comprehensive, unifying approach is needed for public realm, through the use of high quality materials that respond to the function and character of each street and space.

The wide variety of paving materials has proven to be a burden in terms of maintenance. Constraints have led to an approach to public realm in the Town Centre that focuses on trouble shooting such as paving repairs with all pedestrian areas of the Town Centre receiving an annual street clean.

- 
- Yorkstone slabs varying widths
 - Light grey concrete cobbles
 - Concrete setts, light brown, herringbone pattern
 - Buff concrete aggregate slabs
 - Old yorkstone setts
 - Red setts
 - Concrete setts, red/grey colour
 - Granite setts around tree planters
 - Dark Grey setts
 - Tarmac

Plan illustrates the wide variety of paving materials used within the core of the Town Centre

Existing Public Realm Plan

The images below show the many various materials in use and are colour coded to the plan on page 24.



Buff concrete aggregate slabs.



Red setts.



Old Yorkstone setts.



Light grey concrete cobbles.



Dark grey setts.



Concrete setts, red/grey colour.



Yorkstone slabs, varying widths.



Concrete setts, light brown herringbone pattern.



Granite setts around tree planters.

Examples of Rotherham's Public Realm Palette



Attractive pink sandstone (Rotherham Red) of All Saints' Minster, which is also evident in numerous buildings within town (This should be reflected more in the public realm).



Use of seven different surface materials at junction are conflicting creating a disharmonious public realm. Yellow lines appear to be failing on the red pavers.



Yorkstone setts (the old historical palette) used at the transition between materials. Remnants & quirks add richness.



Fan shaped concrete cobbled carriageway with original yorkstone pavements on Church Street.



Clay pavers are a slip hazard in some locations.

Street Furniture

The majority of street furniture in the Town Centre is 'off the shelf', with only a few bespoke elements evident, such as the ornate railings in Effingham and All Saints' Square, and bespoke lighting columns in All Saints' Square. Through the use of similar materials such as black painted steel, wooden slatted seats and a traditional 'Victoriana' style, a generally consistent palette of street furniture has been created. However, in places, this palette is dated and tired, lacking in quality and distinctiveness (especially in relation to heritage areas).

Seating

The majority of benches are wooden, which is popular with the public, and include a wide variety of styles. Older benches throughout the Town Centre are steadily being replaced such as at Market Square. The new benches at All Saints' Square are a positive addition to the streetscene. Informal seating is created by features such as the fountain adjacent to the market, which forms a busy meeting place and various smaller seating areas within streets. The Minster Yard lacks seating opportunities and is an ideal location for enhancements to become a much needed 'lunch time spot'.

Railing

In addition to standard highway railings a number of interesting bespoke railings enliven the public realm. The ornamental railing in All Saints' Square is a high quality example.

Bins

Bins are relatively consistent throughout the Town Centre, and were selected by the public in a design competition.

Bollards

A variety of bollards are used in the Town Centre including the sculptural new bollards at All Saints' Square.

Cycle Racks

Cycle stands are located throughout the strategy area.



Typical bin style found throughout Rotherham Town Centre



Street trees are a positive asset, which are typically in tree grilles.



Abrupt transitions between various treatments.



Dated but functional seating on Bridgegate. Raised planter kerbs, minimise potential damage to trees, but can obstruct pedestrian desire lines and cause trip hazards.



Seating areas such as these in Effingham Square are typical throughout the Town Centre. While they are functional, this type of arrangement can block desire lines on retail streets.



Attractive bespoke railing in All Saints' Square.



Quirky bollards at All Saints' Square.



Recent improvements require continued maintenance.



Bike racks.



Seating adjacent to the river.

Signage, Lighting and Public Art

As with the general street furniture the lighting and signage is predominantly traditional 'Victoriana' in style providing a relatively harmonious appearance, however the condition is poor in some locations. Public art is evident in various locations.

Signage

Signage in the Town Centre can be split into two categories, highway and pedestrian scale. The pedestrian scale signage is typically a traditional design including Welcome, Information and Wayfinding signs.

Lighting

The Town Centre includes a variety of fixtures that fit broadly into the traditional theme. In places fixtures are poor quality and at worst detract from heritage features such as the Minster. Architectural and feature lighting is lacking within the Town Centre. Uplights within All Saints' Square add variety but have encountered reliability and maintenance issues. The Minster should be reinforced as the central focus by lighting.

Public Art

Rotherham Town Centre has a number of public art installations yet this should be expanded. Existing examples include the Bus Interchange mural, underpass art, and sculpture at the Town Hall. Public art adds richness to a space, giving areas their own distinct identity and provides ideal opportunities to encourage public involvement within the public realm. More public art would enhance Rotherham's public realm, adding interest to streets and spaces that provide ownership and civic pride. RMBC are currently advancing this agenda through an emerging Public Art Framework, with a number of exciting projects under development.



Public art outside Town Hall.



Inappropriate signage in heritage area.



Poor quality fixture design adjacent to Minster.



Artwork in underpass.



Traditional lighting fixture along Bridgegate. This fixture has been rolled out throughout much of the Town Centre creating a consistency.



Typical wayfinding signage.



Attractive bespoke railing.



Bespoke lighting columns in All Saints' Square.



Public art outside the bus interchange.



Video screen enlivens space, however is poorly integrated and dominates space.



Information signage located outside the train station.

Trees & Planting

Pockets of Green Space

As mentioned previously the Minster Yard provides the only major green space within the strategy area and forms a green oasis encircling the Minster. This space was originally the graveyard, and when the grave stones were removed it fortunately retained its green character. The Minster Yard is mostly grassed with mature trees including Ash, Willow and Beech. However there is minimal structural planting, with the exception of a few roses. As Rotherham Town Centre lacks quality green space there is a case to reinforce this as a green destination perhaps with planting displays as a main attraction.

Another pocket of green space is the planting buffer adjacent to Effingham Square. This planting is not only successful in how it screens the noise of the busy road, but also exhibits a good variety of planting that adds interest.

Street trees

Within the pedestrian core, street trees have become a key component of the streetscape in Rotherham, particularly Upper Millgate, Frederick Street and Effingham Street. These trees are of vital importance, providing shade and greenery, with species such as Sorbus and Prunus evident. In addition to adding a fledgling green structure to the Town Centre they help define the function of spaces.

Planters

There are a wide variety of planters in the Town Centre however some obstruct pedestrian movement, and are in poor condition. The large planters unlike street trees located in grilles, obscure important views and therefore legibility is decreased.

Threats and Challenges

In general the condition of trees and planting within the Town Centre is good, however street trees have suffered in the past from use of herbicides in paving maintenance. Gaining funding for replacement trees and indeed for additional planting schemes has been challenging due to budget pressures, which is reflected by the lack of examples of planting within the Town Centre. In addition the major new development sites are both a threat and an opportunity in terms of greening. Areas of mature trees will be lost along the river corridor to new development including the Westgate Demonstrator Project placing a major responsibility on delivering a comprehensive approach to greening the Town Centre. Section 4 - Public Realm Vision, sets out a Green Framework which describes the integral role of greening to the future planning and development of Rotherham Town Centre.



Green space buffer adjacent to Effingham Square.



Street trees.



Link to All Saints' Square.



Green corridor evident alongside Riverside.



Mature planting.



Planters obscure views of Minster.



Green approach down Doncaster Gate.

issues & opportunities

Issues & Weaknesses

- **Incohesive public realm**
Tired, dated public realm, with use of too many materials/treatments creating a confused and often detracting appearance. This also creates additional pressure on maintenance.
- **60's & 70's buildings, public space and infrastructure.**
The majority of the north and west side of town is dominated by large scale buildings of varying quality and unwelcoming pedestrian links and spaces.
- **Lack of public art provision**
Bespoke features and art are generally successful but lack in quantity and ambition.
- **Loss of retailers**
Out of town shopping complexes have led to movement of retailers / and shoppers out of the Town Centre. Low quality and disused shop frontages detract from higher quality buildings.
- **Lack of green space**
There is a lack of quality green space and routes in the Town Centre (minimal opportunities for people to relax and enjoy - lunchtime places). Lack of shaded areas and canopies, particularly in All Saints' Square.
- **Barriers**
Movement to the north and west is made difficult due to constrictive road hierarchy, river and railway line. Levels and distance from bus and rail hubs act against areas such as the High Street.
- **Vacant buildings**
The continued vacancy of several shops particularly on High Street, has led to their deterioration.
- **Poor quality Furniture, Signage and Lighting (Clutter)**
Tired and dated in places with some examples of high quality bespoke elements. Uninspiring planters and tree surrounds within the town.
- **Back is turned to the River**
The Town Centre generally turns its back on the River Don and there is little acknowledgement of its importance in the existing townscape.
- **The scale of proposed change**
The proposed major redevelopment planned for the Town Centre is a huge responsibility for RMBC. The ambitious programme for change is a challenge.
- **Parking provision**
The majority of parking is located on the edges of the retail core and pedestrian gateways are often poor.
- **Accessibility**
Pedestrian core is generally accessible, however links into town such as pedestrian bridges and underpasses are poor. Areas such as the High Street and Crofts are a significant distance from the Train Station and Bus Interchange.

Opportunities & Potential

- **Public Realm step change in quality**
A high quality cohesive public realm can transform the environment and be a catalyst for investment in the Town Centre.
- **Transport**
A good infrastructure exists (Train station and bus station) within close proximity of the retail core.
- **High quality buildings**
Rotherham Town Centre benefits from high quality historic and modern buildings, with iconic landmark buildings such as the Minster and Old Town Hall.
- **Views**
Dramatic views are created due to topographical level differences of up to 25 metres. These particularly attractive views out of town look onto countryside, whilst views into the town are of the Minster and other church spires.
- **Street trees**
Good quality street trees in Effingham Street, Upper Millgate and Frederick Street add visual interest, greening spaces whilst providing shade.
- **Current Development Initiatives**
New development provides the opportunity to transform and reinvent vast areas of Town Centre including high quality public realm and public spaces. Infrastructure such as parking must be planned to accommodate intensification of use. High quality public realm is a setting for investment.
- **Public squares/spaces**
Rotherham Town Centre benefits from public spaces such as Effingham Square, All Saints Square, Old Town Hall square and public space outside the market and old mall.
- **River Don and Canal**
Opportunity exists to face the river, exploit views of weir and canal and create an active riverside frontage, continuous riverwalk and destination points at the river edge.
- **Historical street pattern**
The old historical street pattern has mostly been preserved with a majority of the central Town Centre pedestrianised. This has given rise to good pedestrian activity within the retail core.
- **Vibrant Market**
Rotherham Town Centre benefits from a popular indoor market with regular outdoor market weekly on Effingham Street and monthly farmers market.
- **Clifton Park**
Opportunity to improve links to a major green asset located within walking distance of the Town Centre.
- **A reason to visit**
Opportunity for the 'Town Centre Experience' to become reason to visit, especially in the evening. Lighting could become a nationally known feature with interactive displays and the Minster being centre stage. The cultural centre proposed for Forge Island offers an exciting opportunity as a visitor attraction.

what is 'Rotherhamness'?

'Rotherhamness' seeks to capture the essence of what makes Rotherham Town Centre unique and different.

'Rotherhamness' became a commonly used word by groups involved in the development of this strategy.

Consultation with different groups captured a snapshot picture of Rotherham today and of what makes it a unique place to live and visit. Defining 'Rotherhamness' was highlighted by the community as important to help new improvements reinforce the existing character. Proposals shown later in this strategy aim to support this goal.

Thoughts on Rotherhamness have been distilled into the themes below:

- A confident, friendly place
- Attractive, distinctive old buildings
- The community and its history have a story to tell
- The market deserves to remain at the towns heart

"Buildings of historical interest should be renovated and restored."

"Rotherham red sandstone is unique to Rotherham and is used for many of the landmark buildings in the town."

"It is a very friendly place and feels like a typical small English town."

"Rotherham is strong and confident, its something special."

"The town centre already hosts a number of specialist markets."

"All Saints' Minster would be a good focus along with the waterfront - these are real features that could enhance Rotherham's image."

"The markets are good - the best part of the town centre."

"All Saints' Minster is the focal point of this town."

"The holiday activities for children in All Saints' Square are excellent."

"We have wonderful history."



consultation

- Approach

section 3

approach

Introduction

The preparation of the strategy called for wide consultation, in particular, community engagement with 3 specific groups:

- Local interest groups.
- Access groups.
- Outreach/hard to reach groups.

To achieve this aim, a five stage consultation plan was drawn up to bring together the key elements of the consultation processes:

1. Participatory Appraisal.
2. Direct contact with key community groups and organisations.
3. An interactive exhibition of emerging ideas and themes.
4. Statutory Public Consultation.
5. Feedback to local people on the agreed strategy.

1. Participatory Appraisal

Participatory Appraisal (PA) is a technique used to find out about local issues from the people who live and work in a particular area. The process uses a team of trained practitioners and a variety of techniques aimed at engaging people both in the initial stages and in the follow up to community planning and actions. The starting point for PA is that local people are the experts in their own lives and circumstances and that changes taking place will have the best chance of success if they stem from this local knowledge.

On June 1st and 4th 2007, six trained practitioners undertook a participatory appraisal study in Rotherham Town Centre, using a variety of locations both on the street and in larger community buildings. The practitioners engaged with over 250 people representing a cross section of the local community and visitors to the town, including young and older people, those with mobility difficulties and migrant workers. This provided a snapshot of current opinion in relation to public realm issues in the Town Centre and also some wider issues affecting the town. From this exercise it is abundantly clear (from the number of respondents citing them) that the main concerns of those participating in the sessions are the quality of the shops and retail offer generally, personal safety and fear of crime (including related issues of anti-social behaviour), and cleanliness and maintenance of the Town Centre.

2. Direct contact with key community groups & organisations

Working from a comprehensive list of local interest groups, access groups and hard to reach groups, face to face meetings took place to discuss public realm issues, capturing people's views, observations and suggestions. A presentation and paper handouts, containing images of the existing public realm in Rotherham Town Centre were prepared to assist in the discussions.

These meetings raised awareness of some of the issues around public realm and acted as a prompt and guide to encourage people to look at and critically appraise the Town Centre. They also acted as an opportunity to test and verify the findings from the PA exercise and start to prioritise. Additionally, people were helped to make links with the larger physical regeneration activity that is now apparent in the Town Centre as part of the wider renaissance programme and the opportunity to raise awareness of how people can be involved in the programme (through the Town Team) was taken.

Some community groups were particularly helpful and keen to engage fully in the process. The RMBC Access team facilitated a 'public realm' workshop at the Rotherham Disability Forum at which some 28 local people were able to discuss and record issues for people with a range of disabilities. Age Concern Rotherham enabled a number of meetings to be held with older people that were particularly useful in capturing a longer-term perspective of the changes over time in Rotherham Town Centre. Rotherham Youth Cabinet hosted a PowerPoint presentation to over 50 members in the Council Chamber. This led to a group of young people touring the Town Centre with digital cameras and capturing images of 'public realm' through their eyes. They then annotated these pictures with short comments that interestingly were split evenly between positive and negative statements.

Meetings were held at the Unity Centre on several occasions with a range of hard to reach groups. These included people from the Roma community, asylum seekers and a workshop with people with learning disabilities. The latter was also accredited through a written statement as evidence of team working and recognised as a contribution towards their National Open College Network course.

A consultation and information meeting was held with the management committee of Rotherham Civic Society who have a particular interest in Town Centre regeneration and also with the Rotherham South Area Assembly, in whose patch the Town Centre falls.

In total 21 community groups were contacted that resulted in 14 face to face group meetings and discussions with over 350 local people. These were used to inform the emerging themes and ideas that were assembled into the interactive public exhibition display boards.

Contact has been made directly with the following groups:

General Contacts	
Group	Activity
Rotherham South Area Assembly.	Assembly that covers Rotherham Town Centre.
Rotherfed.	Federation of Rotherham Tenants & Residents Groups (TARAs).
Town Centre Manager.	Promotion and development of Rotherham Town Centre.
Rotherham Civic Society.	Concern for preservation and sensitive development in TC.
Voluntary Action Rotherham.	The local CVS supporting, developing and promoting the local voluntary & community sector.
Town Team.	Rotherham Renaissance.
Chamber of Commerce - Business Forum.	In particular contact with Town Centre retailers and businesses.

Hard to Reach Groups	
Group	Activity
Rotherham Ethnic Minority Alliance (REMA). Also the recently formed Ethnic Minority Network.	Umbrella group and focal point Networking forum + opportunity to provide delegates for CEN.
Rotherham Access Audit Group (RAAG)	Independent group with range of disabilities in their membership who carry out access audits.
Rotherham Youth Cabinet – Voice and Influence.	Drawing on membership from local school councils meet regularly to discuss issues of concern.
Rotherham Youth Service.	Youth work.
Age Concern.	Provision of services to older people (55+).
Asylum Seekers.	Handling and housing asylum seekers sent to Rotherham.
RMBC ‘Supporting People’ Team.	Providing service support for the most vulnerable in the community with a range of needs.
Sure Start.	Support services for parents and carers of young children and families.
RMBC Access Service.	Local authority access champion and specialist.
‘Rush House’ Homeless Project.	Accommodation provider.
United Multicultural Centre.	Education, training, access to employment and sports.
Roma Community Group.	Support for 500+ settled Roma Gypsies.
Social Services.	Independent ‘Eastwood Action Group’ who meet on a weekly basis. Self-advocacy mixed group with learning disability.
GROW Giving Real Opportunities to Women.	Personal development and parenting.

3. An interactive exhibition of emerging ideas & themes

Rotherham Open Arts Festival was held between 14th and 23rd September 2007. The venue for this event was the Famous Spiegletent, a temporary pavilion erected in All Saints' Square that provided an unusual and attractive space that was also used for the interactive public realm exhibition. The exhibition was mounted on Thursday 20th September 2007 and was central to a wider display that included up to date information on the Rotherham Renaissance Town Centre development programme and a display of the required flood defence scheme that links with the riverside regeneration plan. The public realm information was represented on eight display boards including one that featured the work of young people from the Rotherham Youth Cabinet. Consultants from Gillespies and officers from RMBC were on hand to assist visitors and discuss any issues arising from the suggested themes and ideas. The feedback gained was considered and integrated in the development of the Public Realm Strategy.

The event had significant pre-publicity with 5,000 leaflets being distributed in the preceding week at the Rotherham Show, press and radio coverage and the Spiegletent itself featuring on regional TV news. The exhibition attracted over 100 people who specifically visited the public realm stand and discussed the proposals with staff. All participants were invited to record their views and comments on a simple questionnaire and in total over 80 were returned on the day. The information provided was analysed and used to inform the evolving design ideas.

The event also proved useful as a networking opportunity. For example links were remade between Rotherham Civic Society and Rotherham Youth Cabinet and interest in collaboration was shown by the 'Unlocking Leadership Project'.

All the local groups who participated in stage 2 of the consultation process were asked to distribute flyers advertising the exhibition and through their individual newsletters, encourage their members to attend and see the emerging public realm ideas and make comments. Of those local people attending on the day, by observation and direct contact, there were representatives from BME groups, parents and carers, access groups, people with mobility difficulties, older people and representatives from the Youth Cabinet.



Spiegletent, All Saints' Square, 20th September 2007, Public Consultation Event

4. Statutory public consultation

The consultation exercise has been devised in accordance with the RMBC consultation protocol, guidance in Planning Policy Statement 12: Local Development Frameworks and The Town and Country Planning (Local Development) (England) Regulations 2004. It will also have regard to the Statement of Community Involvement (SCI) adopted June 2006, and the consultation process will meet the requirements set out in this document.

5. Feedback to local people on the agreed strategy

The final document will be accompanied by a Consultation Statement setting out a consideration of comments received by the Council and changes made as a result.

Application of consultation

The development of this document has included consultation with a number of key groups (see list below) that have provided detailed input into the strategy. This valuable feedback has fed into proposals and culminated in the final draft.

Town Team, Town Centre Planning Group, Rotherham South Area Assembly and Rotherham Civic Society.

Below the table broadly outlines the key themes that resulted from the questionnaires filled at the public consultation event.

Summary of Results from Consultation Questionnaire	
Key themes	
<ul style="list-style-type: none">• The main reason most people visit the town is to go shopping, whilst just over a quarter of those asked work in the Town Centre.• Over half the people asked said they usually visit the town in the morning, at lunch and in the afternoon. Very few people said they visit the Town Centre in the evening.• People said they would be encouraged to visit the Town Centre, if there was a greater range of quality shops, restaurants and pubs, if safety concerns during the evening were addressed, and if more cheap or free parking was available.• Nearly all people agreed with the issues shown on the consultation boards, with utilising the River Don being the most important issue referred to. Concerns about keeping the Town Centre tidy and clean were also expressed.• People generally agreed with the type of street improvements shown on the consultation boards. Some people added comments regarding accessibility of wheelchairs and prams, and again the issue of maintaining a clean town was mentioned.• People were asked what features they would like to see as part of the public realm. Prompts were provided and water features were by far the most popular, public art was thought to be sadly lacking and came second in the 'wish list'. More grassed areas and comfortable seats came third, whilst improving safety within the Town Centre came fourth.• People were asked whether there were any events in Rotherham's history or local people and communities that should be celebrated. A good response suggested the celebration of steelworkers, miners, ethnic groups, Rotherham's cannon making history and design of the screw tap.	<ul style="list-style-type: none">• People were asked what activities and facilities would encourage them to visit the riverside. Prompts were provided and everyone agreed that riverside walks and seating would inspire them to visit the riverside. Nearly all people agreed that cafes and restaurants would be a good idea, whilst festivals and events, art heritage trails and nature and wildlife came third.• People were asked what events and facilities they would like to see in Clifton Park. It was agreed that open air plays and music would be good, with quality cafes. More toilets were also suggested, with better play areas and family & history events. Again it was stated that maintenance and safety issues need to be addressed.• People were asked whether they thought the Rotherham Lighting Experience would attract more people into the Town Centre in the evening. It was thought that generally it would, with ideas such as big displays at Christmas. Concerns were expressed regarding 'carbon footprint' and the creation of a safe environment.• The building which most people would like to see lit was All Saints' Church, with attractive old buildings, the Town Hall and museum also being referred to.

public realm vision

- Bring Harmony to the Town Centre
- Reclaim the Riverside
- Green Framework
- Rotherham Lighting Experience



Based on the baseline and consultation findings demonstrated in the previous sections of the report, the following Objectives, Vision and Framework are proposed for the Rotherham Town Centre Public Realm Strategy:

Why invest in Public Realm?

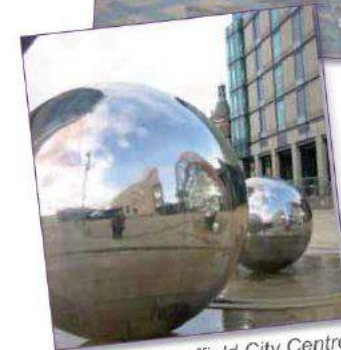
There is a need to bring about a transformation in the quality of the public realm as quality, vibrant streets and spaces form a key component of any successful town or city centre. Creating a distinctive environment helps to attract new footfall, providing a new sense of focus for the community, in turn creating a platform for investment.

Locally, investment in Wakefield and Sheffield, has shown that public realm enhancement can improve an area's competitive position whilst enhancing its heritage value. There are many other examples in Britain and abroad, where recent public realm improvement has been effective.

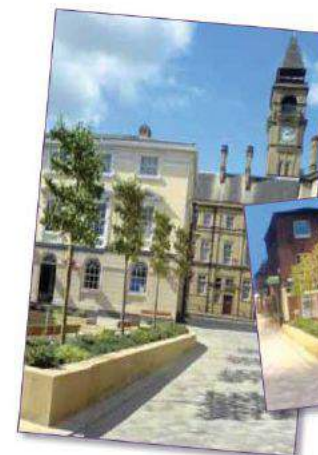
It is also important to recognise that the public realm must evolve to accommodate modern needs. In an increasingly competitive, connected world, where shopping on the internet and carrying out business does not have to take place in our centres, the experience of the place matters more, not less.

The objectives of the public realm strategy are to:

- **Achieve a step change in quality of design that will be embraced by Rotherham.**
- **Create a harmonious, user friendly public realm throughout the Town Centre, which allows for flexibility to respond to local character and functions.**
- **Create a safe, welcoming accessible Town Centre that promotes health and well-being.**
- **Transform resident and visitor perceptions of the Town Centre, re-establishing it as a busy, attractive destination (a family friendly place for the whole of Rotherham).**
- **Create a robust, durable public realm that promotes sustainable maintenance and longevity of capital investment.**
- **Implement a simple, uncluttered streetscape that allows the high quality buildings to become prominent.**
- **Respect Rotherham's heritage and maximise the new opportunities brought by areas of change within the Town Centre.**
- **Add value to the streetscape by providing opportunity for formal and informal activity and events.**
- **Integrate the river and canal into the Town Centre creating continuous public access and destination points.**



Sheffield City Centre.



Wakefield City Centre.

The vision identifies four themes, which are described on the following pages. This is followed by the Framework, which suggests an approach to achieving the vision:

Vision



- **bring harmony to the town centre**

The creation of a cohesive Public Realm Palette of materials, furniture and features, which can lift and tie together the environment.



- **reclaim the riverside**

The creation of continuous public access and destination points along the River Don corridor to reclaim the riverside.



- **a green framework**

The creation of a Green Framework to deliver high quality green spaces and structure.



- **rotherham lighting experience**

The creation of the Rotherham Lighting Experience to promote the Town Centre as a destination.



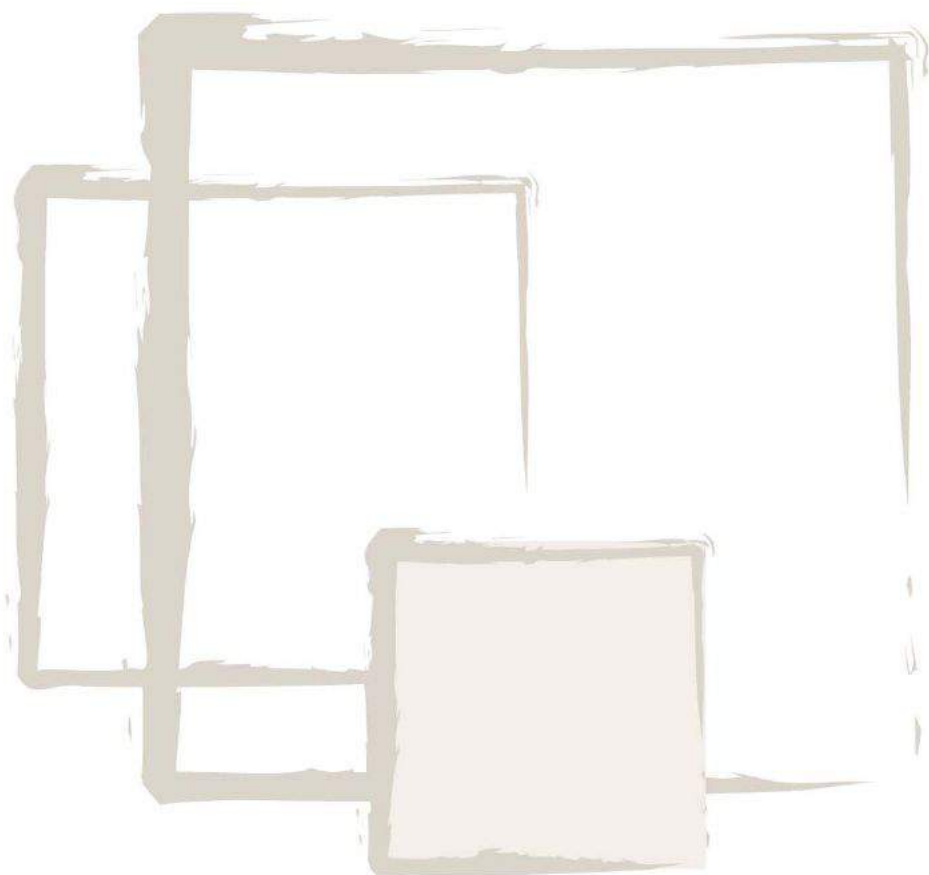
Framework (refer to Section 5)

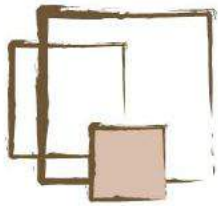
- **spatial hierarchy**

The spatial hierarchy maps the quality and level of investment expected for public realm projects throughout the Town Centre.

- **zones of influence**

How should the vision be applied to Public Realm in different zones of the Town Centre? The Zones of Influence set out project propositions and design guidance.





vision - bring harmony to the town centre

The creation of a cohesive Public Realm Palette of materials, furniture and features, which can lift and tie together the environment.

Goal eight of the Town Charter states " *We want the best in architecture, urban design and public spaces for Rotherham and will no longer accept the mediocre or the banal*". This sets an ambitious challenge for Rotherham.

The baseline identifies the fragmented nature of Rotherham Town Centre's public realm as a key issue. Therefore the first aspiration is to create a **single, identifiable Rotherham Town Centre Public Realm Palette**. The proposed design focuses on a simple, subtle floorscape of buffs, greys and Rotherham Red inspired pinks drawing from proven success and '**Rotherhamness**'. In addition to this understated, elegant canvas, a unifying kerb line/band is proposed that can provide a functional role in defining space, pedestrian/vehicular movement and organisation of street furniture.

For this approach to be successful the palette of materials, furniture and features must allow the flexibility to respond to various locations, both in terms of appropriate quality and distinctiveness. To achieve this, the strategy proposes a suite of three quality levels (Standard, High and Premium). The palette is not intended to be overly prescriptive and should in fact encourage local differences, details and distinctiveness (refer to Zones of Influence p80). For example, two contrasting streets may receive a High Quality Palette, one associated with a contemporary new development and one located in the historic core. The principles in terms of the high quality palette are applied to both however their local distinctiveness should be emphasised by introducing appropriate detailing, finishes, bespoke elements and public art. Therefore the Public Realm Palette can be applied to any given location.

A tool kit approach, to guide public realm treatment:

Step 1

The suite of three quality levels are described through the following **design concepts**, which set **principles for typical scenarios** including traditional vehicular streets, shared surface streets and fully pedestrianised streets and spaces. They provide a co-ordinated approach within each quality level and create continuity in layout and colour where transitions occur between different quality levels. The layouts and demarcation of function is intended to create user-friendly functional streets and spaces. In addition, they suggest general principles for individual elements such as lighting, planting, paving and furniture and how they might interplay.

Step 2

The Spatial Hierarchy (refer to p79) suggests a framework for public realm investment and the map locates **Standard, High or Premium quality levels for each street, space, link and gateway** in Rotherham Town Centre.

Step 3

The Zones of Influence set a **design tone and specific design propositions** suggesting how the public realm treatment can be tailored to various locations in the Town Centre to emphasise 'sense of place' and **distinctiveness** (refer to p80).

Step 4

Further **detailed guidance on each individual element of public realm**, such as paving and street furniture, is shown in the Design Guidelines Section (page 107), culminating in a **Specification Matrix** outlining product performance specification descriptions, for the Rotherham Town Centre public realm palette.



Rotherham's existing public realm.



Standard quality streets, spaces and gateways

The standard palette is practical yet delivers a benchmark in quality. This quality level is based on successful recent improvements to the Town Centre in order to create continuity.

- Concrete Paving Slabs (buff)
- Concrete with Yorkstone Aggregate Conservation Kerbs (buff)
- Concrete Paving Setts for paving accents (Pink/grey mix to match existing improvements)
- Bitmac roadways with concrete paving setts to define key pedestrian crossings and on-street parking bays.
- Standard palette of furniture and lighting.

(Please refer to the public realm specification matrix p132 for specific details.)



Inspiration from other public realm schemes



Inspiration from Rotherham Town Centre.



- ① Scale of light fixtures should respond to scale and function of the street/space. Building-mounted lights could be used where appropriate in Standard Quality areas.
- ② Buff concrete paving slabs to be used for majority of paved areas (which are not trafficked).
- ③ Contrasting concrete paving setts can be used to define a change of function such as highlighting vehicle use areas, on-street parking bays, pedestrian crossings and public spaces.
- ④ A variety of seating opportunities should be provided along streets (where appropriate), and within public spaces. Layout of furniture should be linear to minimise street clutter and avoid pedestrian desire lines.
- ⑤ Greening should be an integral component of the streetscape, where appropriate. Its application should be guided by the Green Framework (Refer to p63).
- ⑥ Concrete aggregate kerb with contrasting paving sett band to define function and delineate space.

Design concept:

(The following concept sets out an approach to a variety of typical scenarios. Please refer to Section 6, Design Guidelines for more detailed principles.)





High quality streets, spaces and gateways

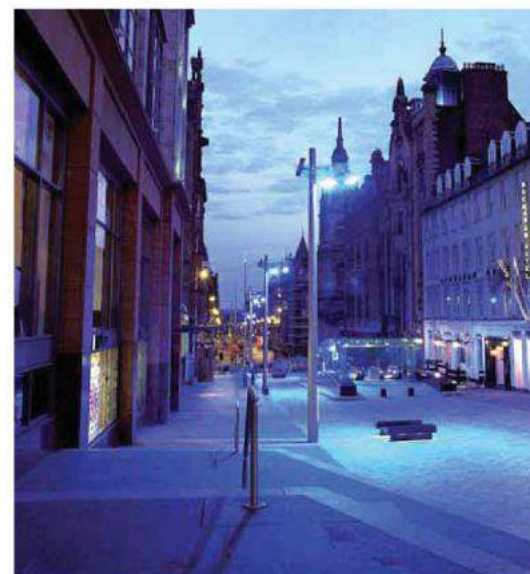
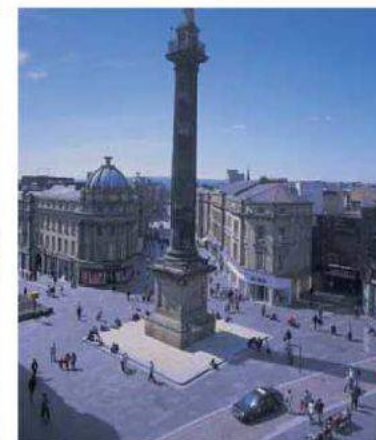
A high quality approach for Rotherham's major streets, spaces and gateways. This quality level is inspired by the recent improvements in All Saints' Square.

- Yorkstone paving slabs (deep grey/ochre).
- Grey Granite Kerb to create strong feature.
- Natural Stone Paving Setts for paving accents (Grey/buff Yorkstone or Rotherham Red Sandstone).
- Heavy traffic carriageways should receive Bitmac (with granite or slag aggregate). Granite Paving Setts (Rotherham Red/Grey mix) should be used to upgrade light traffic carriageways, pedestrian crossing and on-street parking bays.
- Standard palette of furniture and lighting to maintain cohesive approach (see Standard Palette), however site specific variations should be introduced to create distinctiveness (see examples).
- Incorporate art and features (see examples).

(Please refer to the public realm specification matrix p134 for specific details.)



Recent improvements to All Saints' Square



Inspirational images of public realm improvements from throughout the UK

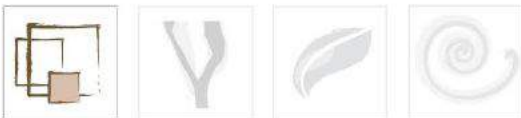


- ① Scale of light fixtures should respond to scale and function of the street/space. The standard quality fixtures will be appropriate in many cases for continuity, however it may be necessary to introduce variations to create distinctiveness in certain locations. Building-mounted lights and accent lights such as floor lights should be integrated where appropriate.
- ② Natural stone paving slabs for majority of paved areas to create a simple, elegant floorscape.
- ③ Contrasting natural stone paving setts used to define a change of function such as highlighting on-street parking bays, pedestrian crossings and public spaces. The design of high quality areas should encourage pedestrian dominant environments.
- ④ A variety of seating opportunities should be provided. Layout of furniture should be linear to minimise street clutter and avoid pedestrian desire lines.
- ⑤ Greening should be an integral component of the streetscape, where appropriate. Its application should be guided by the Green Framework (refer to p63). High Quality areas should incorporate a greater variety of features including raised planters and more innovative solutions such as green walls.
- ⑥ Public Spaces should include a high quality focal point and a variety of seating opportunities (formal and informal). Spaces should be designed with function in mind for both day to day use and more occasional use such as events or markets.
- ⑦ Grey granite kerb to define function whilst providing a strong feature that unifies the scheme.

Design concept:

(The following concept sets out an approach to a variety of typical scenarios. Please refer to Section 6, Design Guidelines for more detailed principles.)





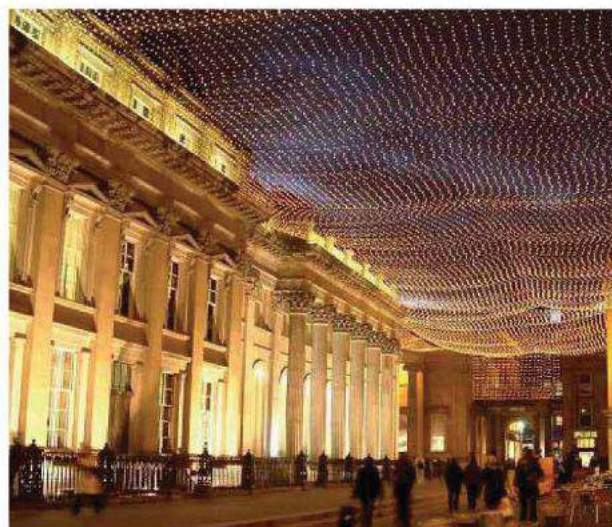
Premium quality streets, spaces and gateways

A showcase for Rotherham intended for a select few streets and spaces. (National/International profile). This quality level proposes a step change in terms of both investment and design.

Same approach/palette as High Quality Streets, Spaces and Gateways, however additional investment should deliver:

- Increased site specific detailing (such as introduction of additional materials into the public realm).
- Bespoke furniture and lighting (see examples).
- Landmark art and features (see examples).

(Please refer to the public realm specification matrix p136 for specific details.)



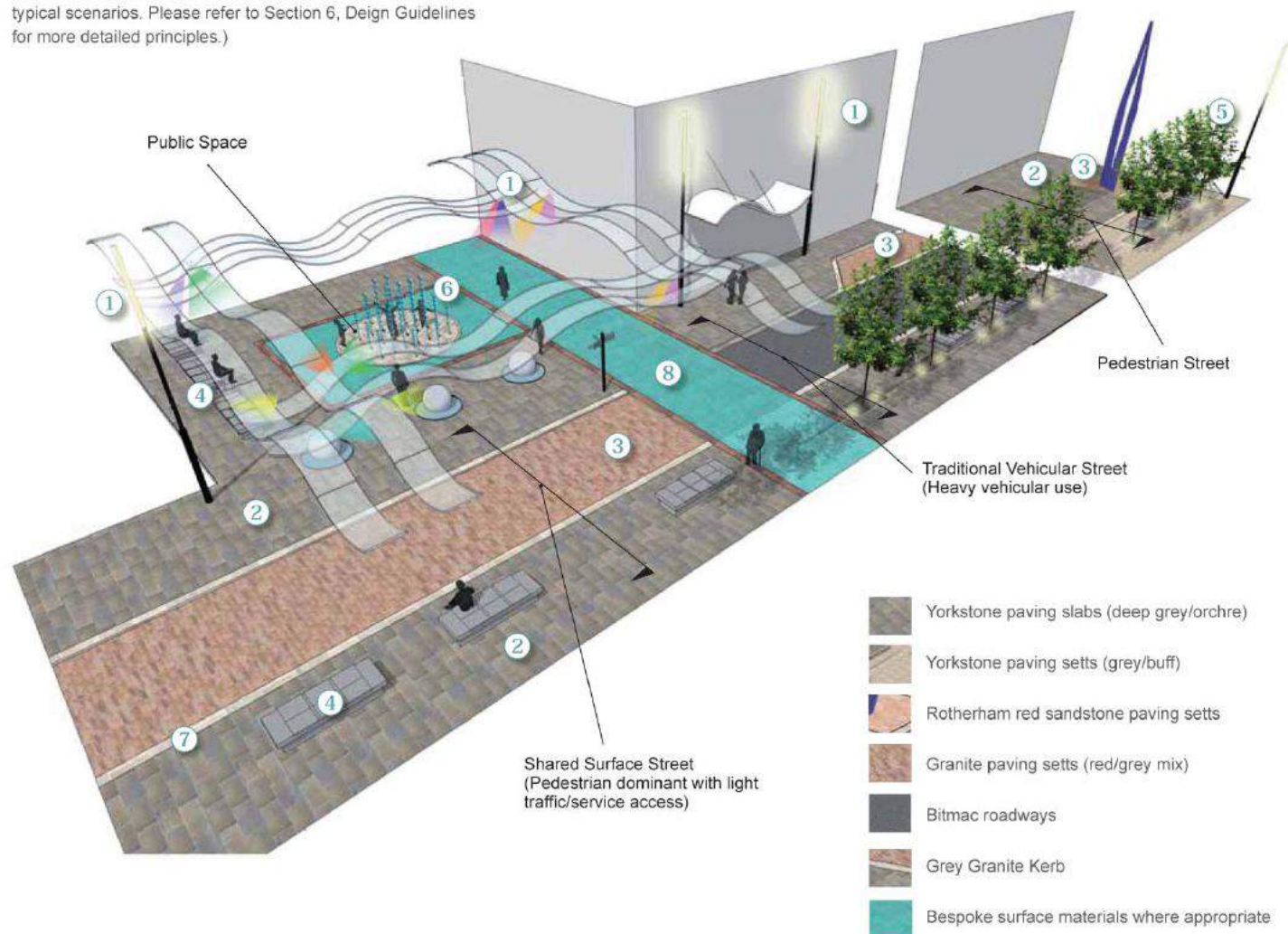
Exchange Square, Glasgow

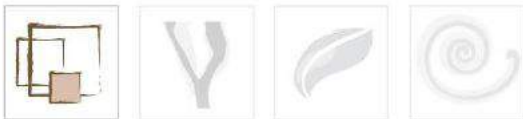


- ① Scale of light fixtures should respond to scale and function of the street/space. Lighting should be a major feature of the public realm and be bespoke and innovative throughout. Building mounted lights and accent lights such as floor lights should be integrated where appropriate.
- ② Natural Stone paving slabs for majority of paved areas to create a simple, elegant floorscape.
- ③ Contrasting natural stone paving setts used to define change of function such as highlighting on-street parking bays, pedestrian crossings and public spaces. The design of premium quality areas should encourage pedestrian dominant environments and where appropriate introduce new surface materials such as recycled glass or bold paving bands.
- ④ A variety of seating opportunities should be provided and be bespoke throughout. Layout of furniture should be linear to minimise street clutter and avoid pedestrian desire lines.
- ⑤ Greening should be an integral component of the streetscape, where appropriate. Its application should be guided by the Green Framework (refer to p63). Premium Quality areas should incorporate a greater variety of features including raised planters and more innovative solutions such as green walls.
- ⑥ Public Spaces should provide a landmark focal point (such as canopies, structures, lighting, public art, water feature) and a variety of seating opportunities (formal and informal). Spaces should be designed with function in mind for both day to day use and more occasional use such as events or markets.
- ⑦ Grey granite kerb to provide a functional feature that unifies the scheme.
- ⑧ Opportunities to introduce paving elements that differ from the High Quality Surface Materials to create unique and dramatic spaces.

Design concept:

(The following concept sets out an approach to a variety of typical scenarios. Please refer to Section 6, Design Guidelines for more detailed principles.)



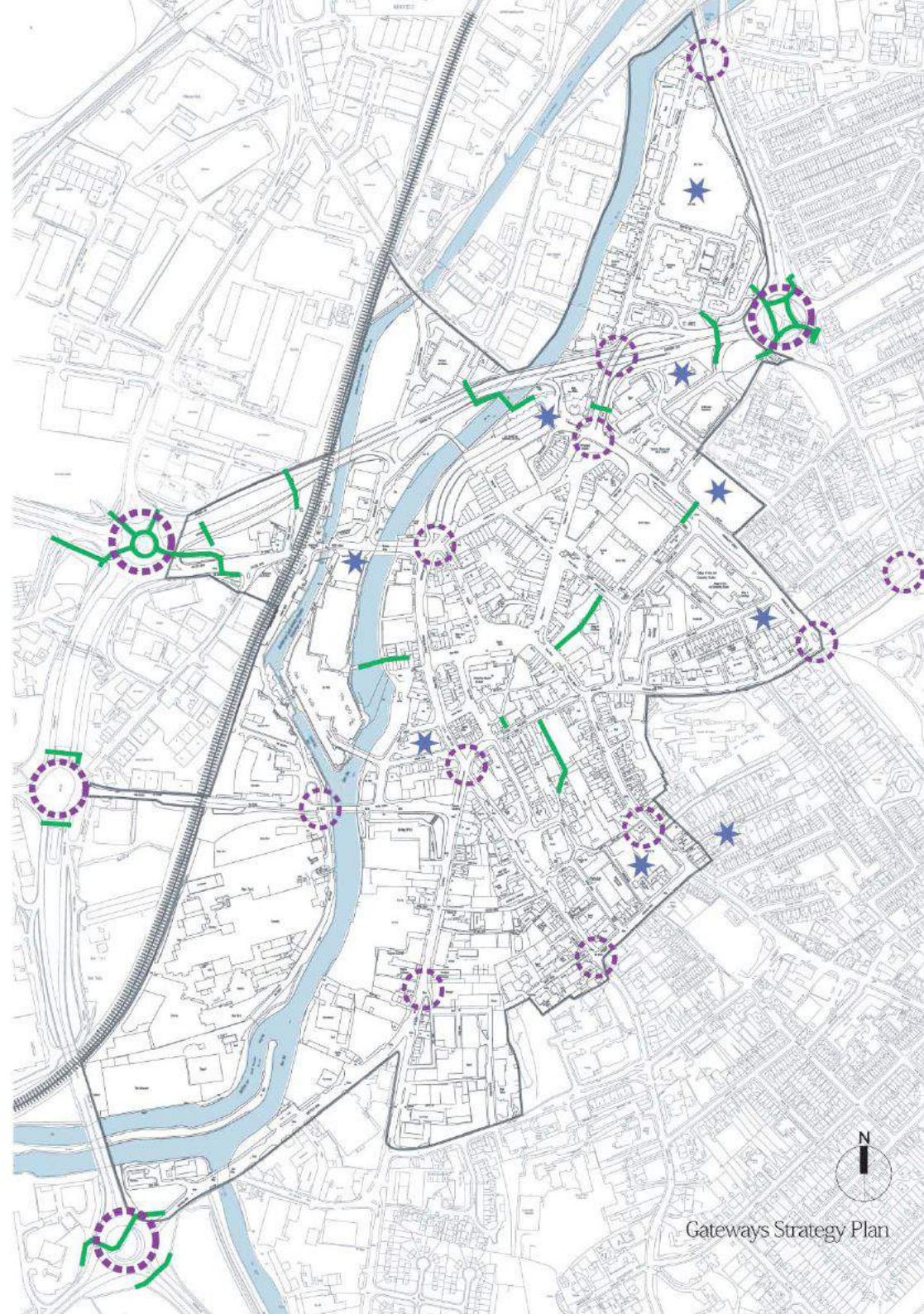


Gateways to provide a positive, welcoming first impression and announce arrival in Rotherham Town Centre.

Gateways should:

- Follow the Premium, High and Standard Palette's listed above for surface materials, furniture, lighting and features as defined by the Spatial Hierarchy Plan (refer to p79).
- Gateways and features should respond to their townscape and landscape context but be unified to promote a distinctive arrival into Rotherham using colour (Rotherham red) and light (tie in with Rotherham Lighting Experience initiative refer to p73).

-  Roads and Roundabouts
-  Confined Routes
-  Car parks & Public Transport Gateways



Gateways Strategy Plan



Confined Routes

Underpasses, Alleyways and Pedestrian Bridges

A number of arrival routes into the Town Centre require pedestrians to walk through confined spaces such as underpasses, alleyways and pedestrian bridges. These routes can feel enclosed and unsafe, as the lack of active frontages and poor lighting can create a negative pedestrian experience. The majority of underpasses currently incorporate public art, which should be developed more. However the overall environment of these routes remains poor and unwelcoming.

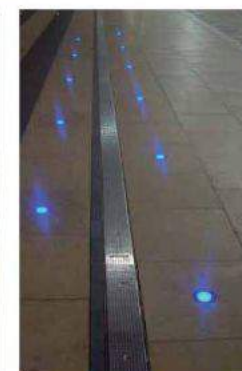
Approach

- Illuminate these routes imaginatively.
- Interactive public art.
- 'Dead end' views should be transformed into clear views of activity to improve the pedestrian experience.
- Encourage active frontages.
- Well-lit to decrease uncertainty.
- Clear wayfinding to name desired pedestrian routes.

Existing Town Centre



Inspiration images



Pedestrian links and destinations should be announced by night, which removes uncertainty.



Car Parks & Public Transport Gateways

Surface car parks are generally on the edges of the Town Centre and in some cases feel disconnected from destinations. Generally the car parking areas and pedestrian routes to them are purely functional and in poor condition.

Railway Station and Bus Interchange pedestrian gateways open onto vehicular dominant places. However links from the Bus Interchange onto Frederick Street are more positive.

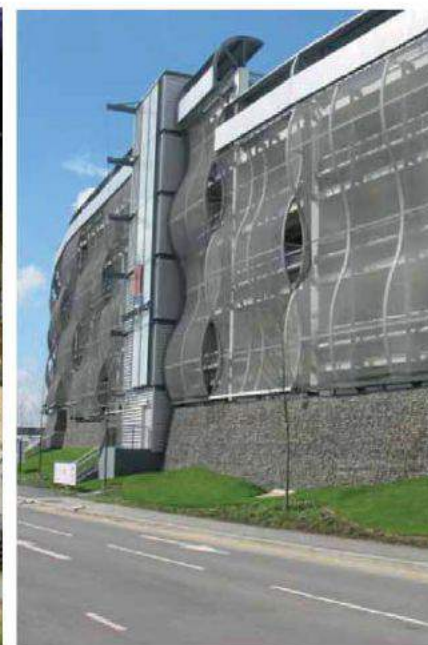
Approach

- Car parks and public transport gateways should welcome visitors to the Town Centre.
- Car parks and public transport gateways should be laid out in a legible manner, well overlooked and lit to create user friendly safe environments.
- Multi-storey car parks can become positive pieces of townscape and landmarks. They are important and often underestimated first impressions of a place.

Existing Town Centre



Inspiration images



Well lit, attractive car parks.

Roads & Roundabouts

Main Roads into the Town Centre are generally vehicular dominant and lack distinctiveness. However glimpsed views of landmarks such as All Saints' Minster and high quality buildings along Moorgate and Westgate create a sense of place.

The series of roundabouts that circle the Town Centre are landscaped but do not take advantage of their potential impact.

Approach

- Major Roadways and linear gateways such as Centenary Way can be treated boldly with repetitive features such as tree avenues, sculptural elements and dramatic planting schemes.
- Roundabouts and key junctions should become a stage for landmark public art, lighting and landscaping.
- New development can mark an arrival point along a route.

Existing Town Centre



Potential Gateway Improvement

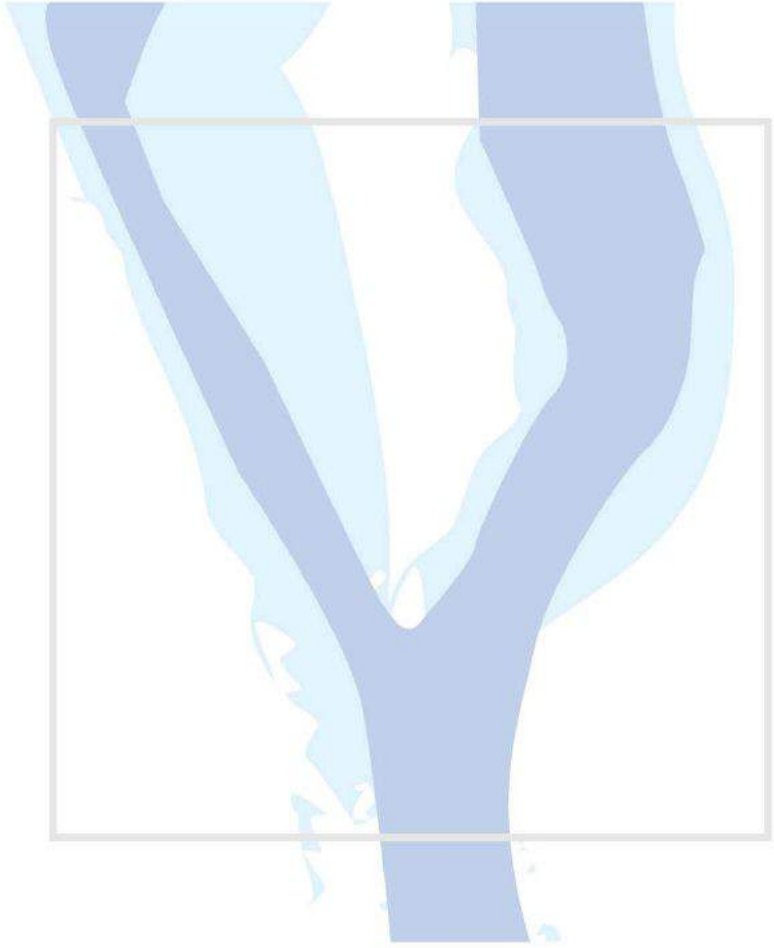


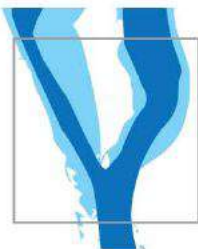
Potential Gateway approach 'Centenary Way'.

Inspiration images



Bold public art, lighting and landscaping.





vision - reclaim the riverside

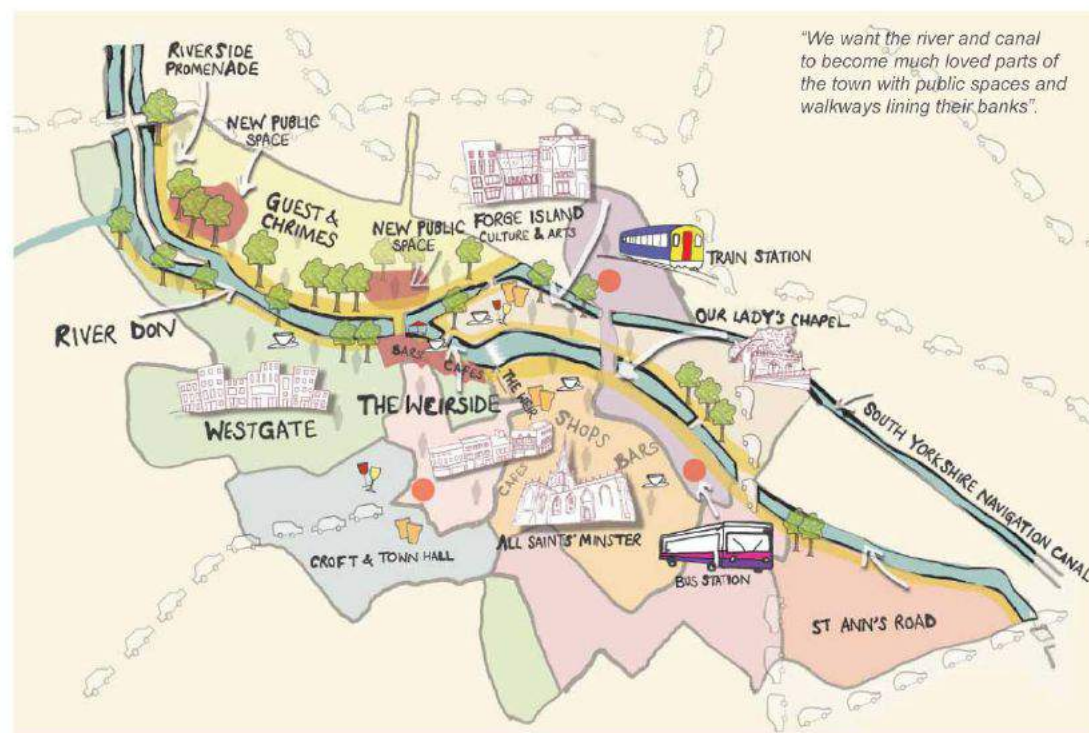
The creation of continuous public access and destination points along the River Don corridor to Reclaim the Riverside.

The River Don is currently a hidden asset, which lacks public access and is addressed by backs of buildings. **Rotherham must reclaim and celebrate the River Don.** The planned major redevelopment of sites adjacent to the waterfront including Guest & Chrimes, Westgate and Forge Island, creates a once in a lifetime opportunity. Goal one of the Town Charter states "We want the river and the canal to form a key part of the town's future. We want the river and canal to become much loved parts of the town with public spaces and walkways lining their banks".

The strategy aims to provide:

- Continuous public access along the riverside, which passes through a number of distinctive areas and provides an attractive, safe, accessible experience/environment.
- Attractive links to and from the riverside - tie in with main routes and destinations in the Town Centre.
- A series of river icons and destinations along the corridor. Emphasise existing icons such as the Weir and Our Lady's Chapel and create a number of new public spaces that interact with the river, in particular in relation to development of Guest & Chrimes site.
- A public art attraction transforming the potentially negative flood defence walls into a creative canvas subject to agreement from the Environment Agency.

The Rotherham Riverside Experience



The map illustrates the end goal vision that the Rotherham Riverside becomes a leisure destination and an integral part of experiencing the Town.



Strategy

Large sections of the riverside currently have no pedestrian access and where access does exist the pedestrian environment is often poor with backs of buildings facing onto the river (refer to Baseline Section for further details, p19).

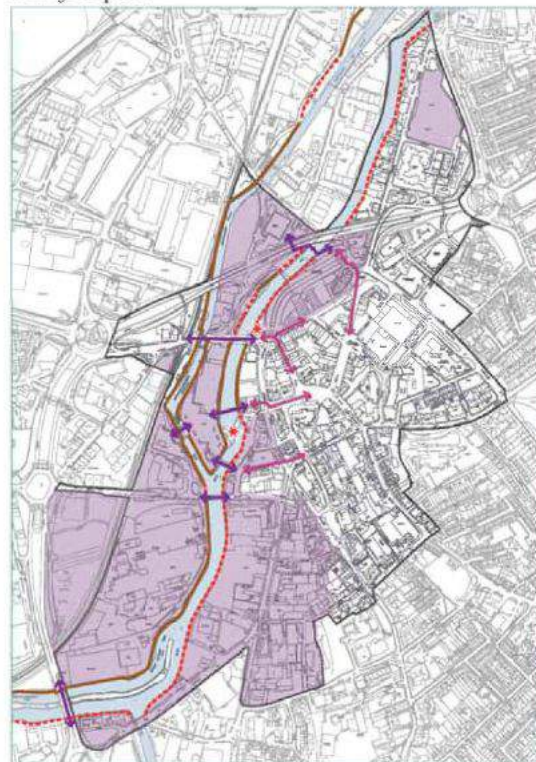
The strategy plan sets out a framework to deliver:

- continuous riverside access.
- a series of river destinations and icons.
- a series of upgraded and new bridges which increase connectivity and importantly create landmark features.
- a riverside well connected to the wider Town Centre.

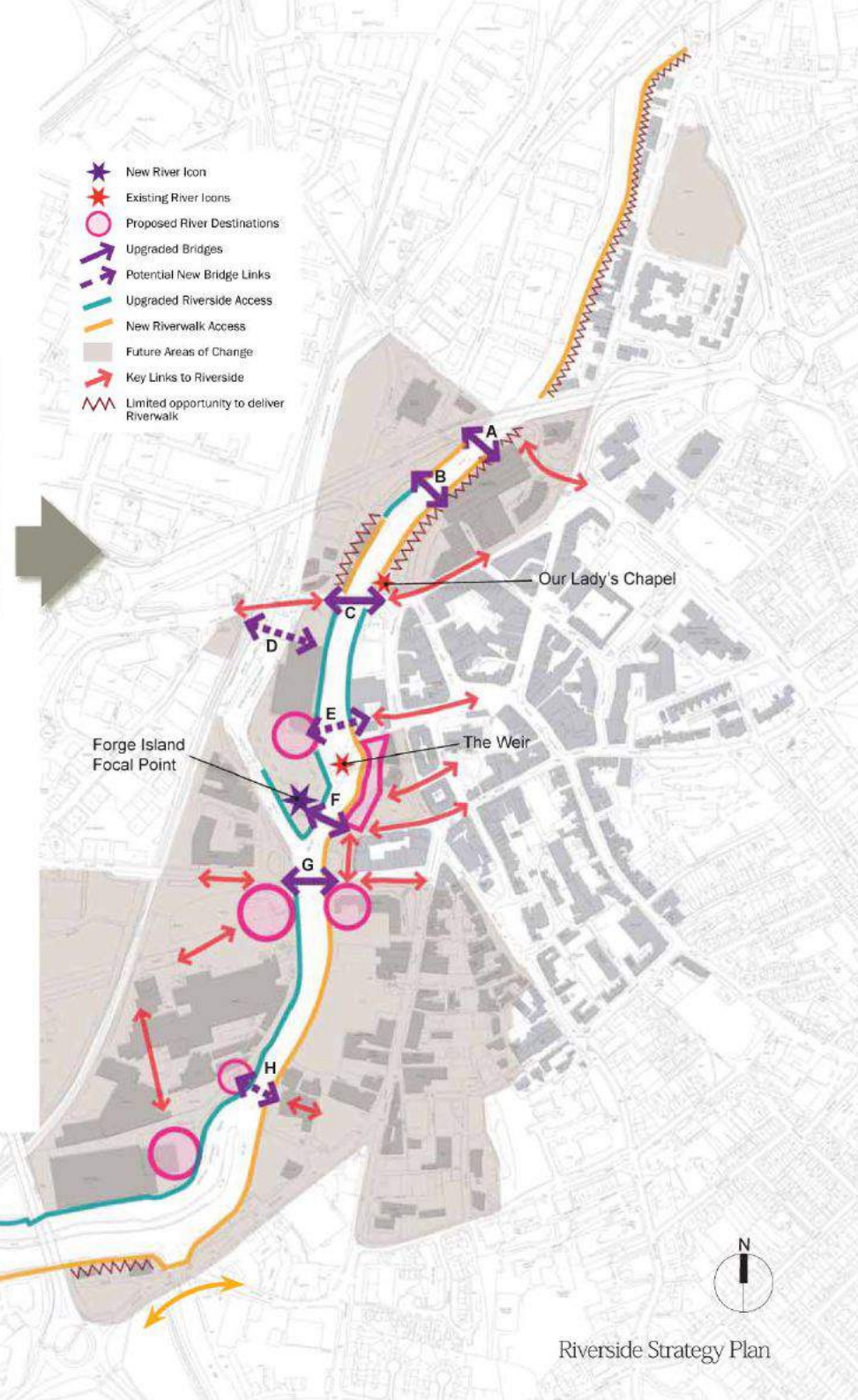
The strategy plan considers the potential for improvements to existing bridges and possible new bridge links to cross the River Don :

- Opportunity to upgrade / replace existing pedestrian bridge as part of improved riverside access (long term).
- Existing vehicular access to multi-storey car park. Long term aspiration to improve if crossing remains in longer term.
- Existing landmark bridge adjacent to Our Lady's Chapel. Opportunity to refurbish and install feature lighting.
- Aspiration for new pedestrian bridge to link train station and Forge Island Cultural Centre.
- Existing poor quality covered pedestrian bridge. Opportunity for new landmark link to Forge Island.
- Existing vehicular access to car park. Opportunity to remove or replace if required as part of redevelopment of Forge Island.
- Existing landmark bridge. Opportunity to refurbish and install feature lighting.
- Potential need for new bridge to link with Guest & Chrimes redevelopment. Need and location to be established through further investigation.

Analysis plan



- Existing Pedestrian Bridge Crossings
- Existing Riverside Access
- No Riverside Access
- River Don and Canal corridor
- Future Areas of Change
- River Icons
- Pedestrian links to river



WETLAND

Riverside Strategy Plan



Riverside Public Realm

A continuous and united riverside which responds to its surroundings

Public Realm along the riverside should follow the Rotherham Town Centre Public Realm Palette to create continuity (refer to p43). The Spatial Hierarchy (refer to p79) sets out the proposed limits of Standard, High and Premium Quality Public Realm along the riverside. In addition the public realm treatment should be responsive to various locations along the riverside, such as providing a sensitive heritage approach to Our Lady's Chapel and a more contemporary approach for the Westgate Demonstrator.

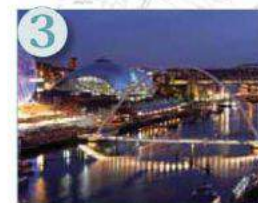
In order to promote the riverside as its own place, the strategy proposes the introduction of a unique element that should run throughout acting as a uniting feature. Options include:

- A 'Rotherham Riverside' logo/icon incorporated into banners, signage and bespoke furniture.
- A 'Rotherham Riverside' bespoke railing.
- Educational/wayfinding signage.
- Themed artwork such as treating the flood defence wall as an interactive art installation, subject to agreement from the Environment Agency.

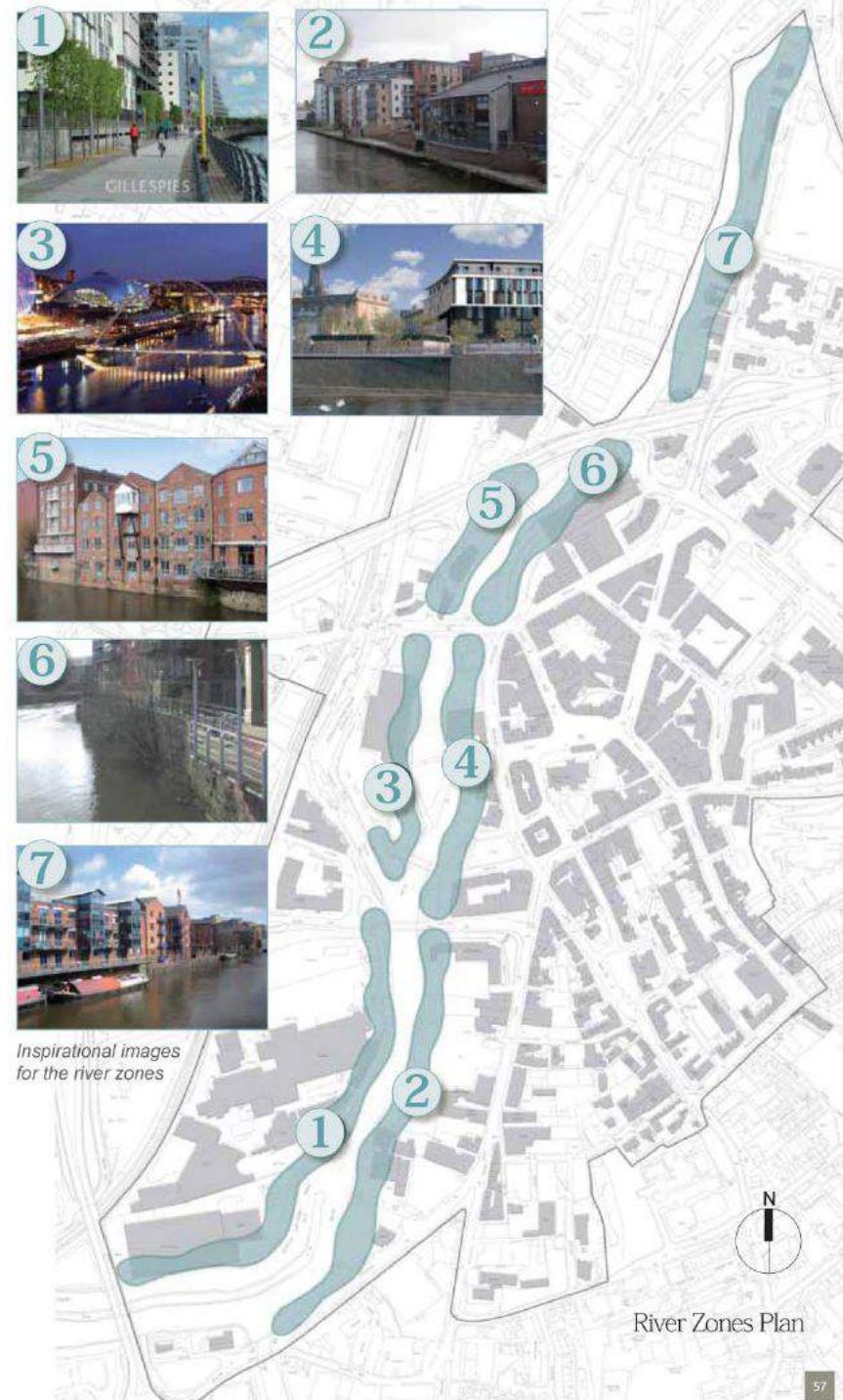
River Zones Plan

The strategy outlined in the previous pages describes the fundamental goal to deliver high quality continuous public access to the River Don. It is apparent that some stretches of the riverside will be more deliverable than others, with a number of early win opportunities emerging. However, in addition to seizing the initiative and building on early momentum on these opportunities, broad buy-in to the wider aspirations must be gained. Within this framework, delivering the more challenging locations can become integral to future plans and part of development site requirements. This approach can draw many parallels to the regeneration of a major street, which often requires a systematic programme of improvements over a long period. The River Don can aspire to be a vibrant place within Rotherham Town Centre's urban grain.

The plan and table address each zone of the riverside in terms of the key challenges to delivering high quality public access and begins to set out some possible solutions.



Inspirational images for the river zones



River Zones Plan



River Zones Approach

River Zone	Description	Challenges	Possible Solutions
1	<ul style="list-style-type: none"> Trans Pennine Trail runs along southern and eastern boundary of Guest & Chrimes site. Riverside to be improved in association with future redevelopment of Guest & Chrimes Site 	<ul style="list-style-type: none"> Potential need for significant flood alleviation measures Challenging site to deliver continuous pedestrian access along riverfront 	<ul style="list-style-type: none"> Where feasible deliver public access along the riverside Create a public destination point adjacent to the river.
2	<ul style="list-style-type: none"> Currently no formal riverside access. Riverside promenade proposed in conjunction with future redevelopment of Westgate Area (Design Code for Rotherham Town Centre River Corridor). 	<ul style="list-style-type: none"> Longer term development proposal (river is currently backed onto by existing buildings). Design approach should be cohesive and interactive with Guest & Chrimes promenade. 	<ul style="list-style-type: none"> Actively pursue opportunities to face new development onto the riverside. Riverside treatment scenario B, C & D are the preferred solutions.
3	<ul style="list-style-type: none"> Existing riverwalk adjacent to Tesco. Riverside to be improved in association with Forge Island redevelopment which is a long term aspiration. 	<ul style="list-style-type: none"> Longer term proposal with no firm proposals or guidance. Location of new pedestrian bridge to Westgate Demonstrator is uncertain (existing bridge is poor quality). 	<ul style="list-style-type: none"> Create a clear vision at the outset, which identifies the riverside as a key feature. The corner of Forge Island should be highlighted as an opportunity for a landmark feature (River Icon - refer to p114 for further details). Riverside treatment scenario B & C are the preferred solutions.
4	<ul style="list-style-type: none"> Current plans to improve riverside access as part of the Westgate Demonstrator Proposals. This includes a high quality public space called the 'deck of cards'. 	<ul style="list-style-type: none"> High aspirations due to this being the first phase of improvements. Gap in time between delivery of Westgate Demonstrator riverwalk and future improvements to other sections of the river. 	<ul style="list-style-type: none"> Deliver the 'deck of cards' public space as an exemplar for future projects and continue to advance adjacent projects within the wider framework. Riverside treatment scenario B, C & D are the preferred solutions.
5	<ul style="list-style-type: none"> Limited existing access adjacent to Trades Club. Area likely to be redeveloped in the future following the relocation of RMBC offices to Guest & Chrimes. 	<ul style="list-style-type: none"> No short term opportunities to create continuous riverside access. Area is currently dominated by back of Bus Station and access to car park. 	<ul style="list-style-type: none"> Improve existing pedestrian bridge and underpass. Build in requirement to deliver riverside access as part of any future redevelopment. Riverside treatment scenario C & D are the preferred solutions.
6	<ul style="list-style-type: none"> Access is blocked at the back of the bus station (no riverside access). Improvements to the bus station are proposed. 	<ul style="list-style-type: none"> Access is blocked due to potential conflict with bus service areas. Therefore no existing space is available to deliver riverwalk. 	<ul style="list-style-type: none"> If proposals for bus station improvements advance, a requirement should be built in to deliver riverside access along this stretch. If bus station proposals cannot deliver an 'on land' solution, a cantilevered walkway is a potential solution to explore. Riverside treatment scenario D & E are the preferred solutions.
7	<ul style="list-style-type: none"> Currently no riverside access. 	<ul style="list-style-type: none"> River is currently backed onto by a number of buildings. No current redevelopment proposals to support introduction of riverside access. 	<ul style="list-style-type: none"> Negotiations with individual landowners to acquire space for riverwalk, which may be a lengthy piecemeal approach. Build requirement to deliver riverside access as part of any future redevelopment. St. Anne's Leisure Centre brings a new use to the north of the town. Riverside treatment scenario D & E are the preferred solutions.



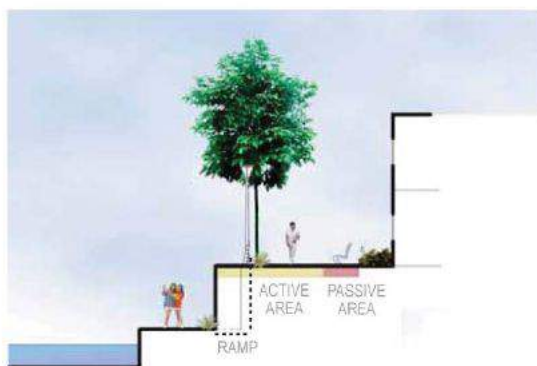
Riverside Treatments

As described in the River Zones Approach, a number of different constraints and opportunities present themselves dependent on the location. The following diagrams illustrate typical scenarios ranging from wide pedestrian/vehicular boulevards to cantilevered walkways, in order to deliver continuous riverside access.

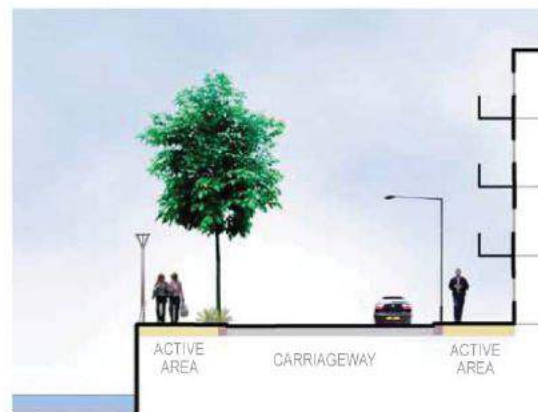
Scenarios A and B should be encouraged as a minimum for all new development offering vehicular or non vehicular applications. These can be reorganised to create a two level walkway in locations where the flood wall height requirements are significant or where a lower level is deemed important for ecological or leisure use as shown in Scenario C.

Scenario D illustrates a treatment where buildings cannot be setback either due to new development site constraints or through being an existing building. Where possible the lack of landscape space can be offset through river level planting. New buildings should be required to provide a setback to accommodate public access to the riverside, however in a number of locations existing buildings directly abut the river.

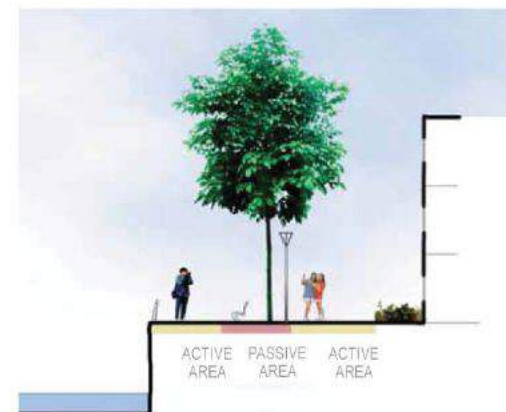
Scenario E illustrates a cantilevered walkway, which may be necessary to achieve the wider aims of a continuous riverside access. This would be subject to structural condition of existing riverwalk and necessary approval from British Waterways and the Environment Agency (refer to Westgate Demonstrator Strategy pgs 16, 18, 21 & 23).



C. Terraced Pedestrian riverwalk.



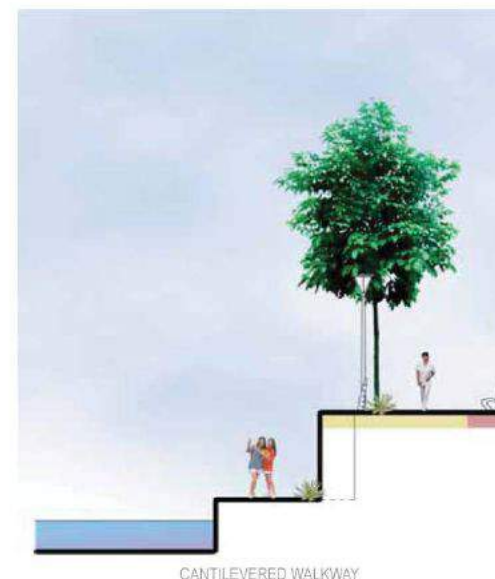
A. Pedestrian/Vehicular Boulevard.



B. Pedestrian Boulevard.



D. Pedestrian Riverwalk



E. Cantilevered pedestrian riverwalk.



The River Wall should become a canvas for public art to transform it into a visitor attraction

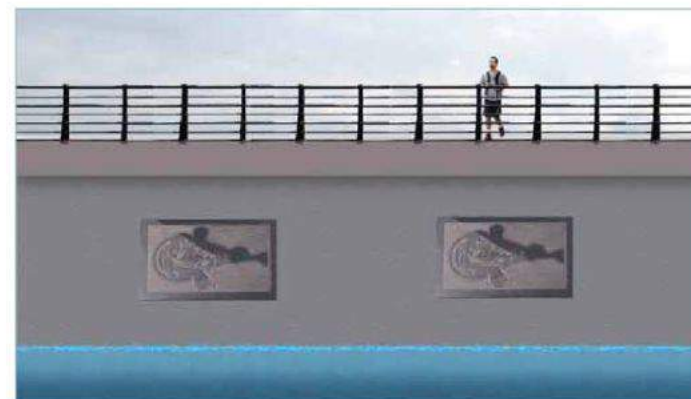
The river wall currently dominates the river environment as the River Don passes through much of the Town Centre. Its required improvement as part of the RMBC Flood Alleviation Scheme provides an opportunity to enhance the river wall treatment, both from a functional and aesthetic point of view.

A major design/art competition is proposed to tackle the river wall. The following design ideas provide inspiration for RMBC to take forward into the design competition. The brief should explore the following two scenarios:

1. An understated approach along the majority of the river creating the opportunity for 'big bang' art installations at premium locations such as Forge Island. A bold wall coping with ornamental railing can run continuously to create unity throughout, subject to agreement from the Environment Agency.
2. The entire river wall as a continuous high impact public art installation throughout the Town Centre. A landmark attraction for Rotherham.

1. Understated solutions:

- possible opportunities to improve low key areas of river walk.
- terraced riverside walk to increase interaction with river, provide opportunities for wetland planting.



Concrete/Natural Stone Coping and concrete wall with art insets.



Engraved Concrete/Natural Stone Coping and concrete wall with insets/texture to create interest.



Natural Stone Coping with inset lighting and wall with Rotherham Red cladding.



Terraced Pedestrian Riverwalk.



2. High impact solutions:

- high impact solutions that could be applied as 'big bangs' or continuously along the whole riverside.

Wall Treatments

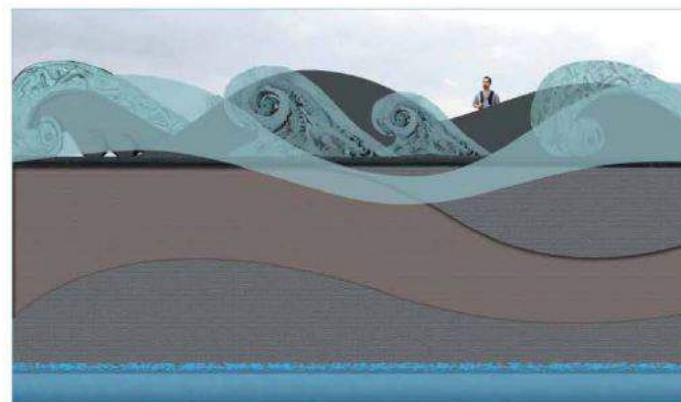
- **Text, Poetry, A Story** - Which winds its way along the river wall or reveals more as the water levels change (local writers, text messaging, design competition).
- **A Fairy Tale Concept** - 'A Subterranean Magical World' living in Rotherham's river walls.
- **Biodiversity** - Pockets and ledges for natural regeneration & wildlife.
- **Evolving Wall** - Materials and textures which evolve and vary with time and respond to the water level.
- **An Art Wall** - Street art, lighting, texture or colour.

River Features

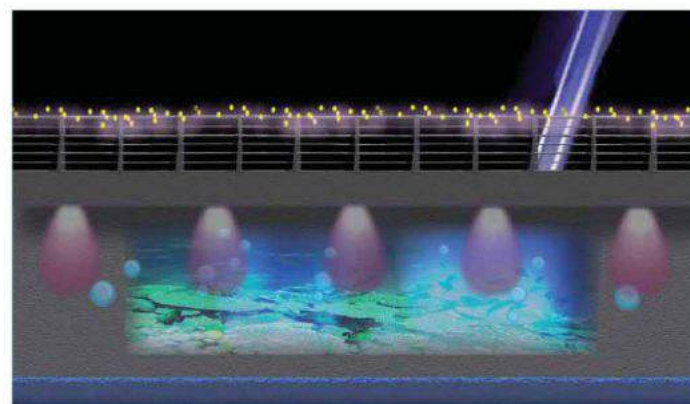
- **A Water Wheel Generator** - A feature at the Weir (historic precedent) that powers features along the river.
- **Moving Light** - generated by activity, water flow etc.
- **Reflection.**
- **Whimsical Sculpture** - The River Don 'monster'!
- **Historical Sculpture** - Inspired by Forge Island cannon making industrial history.
- **Floating Sculptures or Landscapes.**
- **Visitor Attractions** - Features or buildings built into the river wall such as the 'faux boat' at Portmeirion, North Wales.

Crossing the River

- **Light Show** - lasers that cross the river back and forth.
- **Bridging Sculptures** - A variety of features that cross the river.
- **Water Motion** - jets, water walls powered by the water wheel.



Sculptural riverside environment.



Opportunities for art and projections.



Antony Gormley installation where the rising tide of the sea interacts and covers sculptural figures.





vision - green framework

The creation of a Green Framework to deliver high quality green spaces and connections

Rotherham Town Centre's environment benefits from an existing green structure, including street trees in many of the pedestrianised areas and Clifton Park being located within walking distance. However development in recent years has led to some losses in green structure and therefore the strategy aims to mitigate this as well as expand the existing provision. This can be achieved by strengthening the green structure into a framework of green spaces, pocket parks and connections.

The strategy brings numerous environmental benefits including enhancing urban wildlife and cooling the urban environment in response to climate change. Building on Rotherham's current Sustainability Framework, the Council are developing a borough wide Environment and Climate Change Action Plan.

In addition it brings the opportunity to introduce new functions to widen the appeal of the Town Centre including 'lunch time spots' and play areas for children. Improvements to the green framework will play an important role in Rotherham's health and well being agenda and green space improvements should set a new benchmark in quality and where appropriate seek national recognition through initiatives including the Green Flag Awards. The following plan illustrates the principles of a successful green framework.

Major Green Spaces

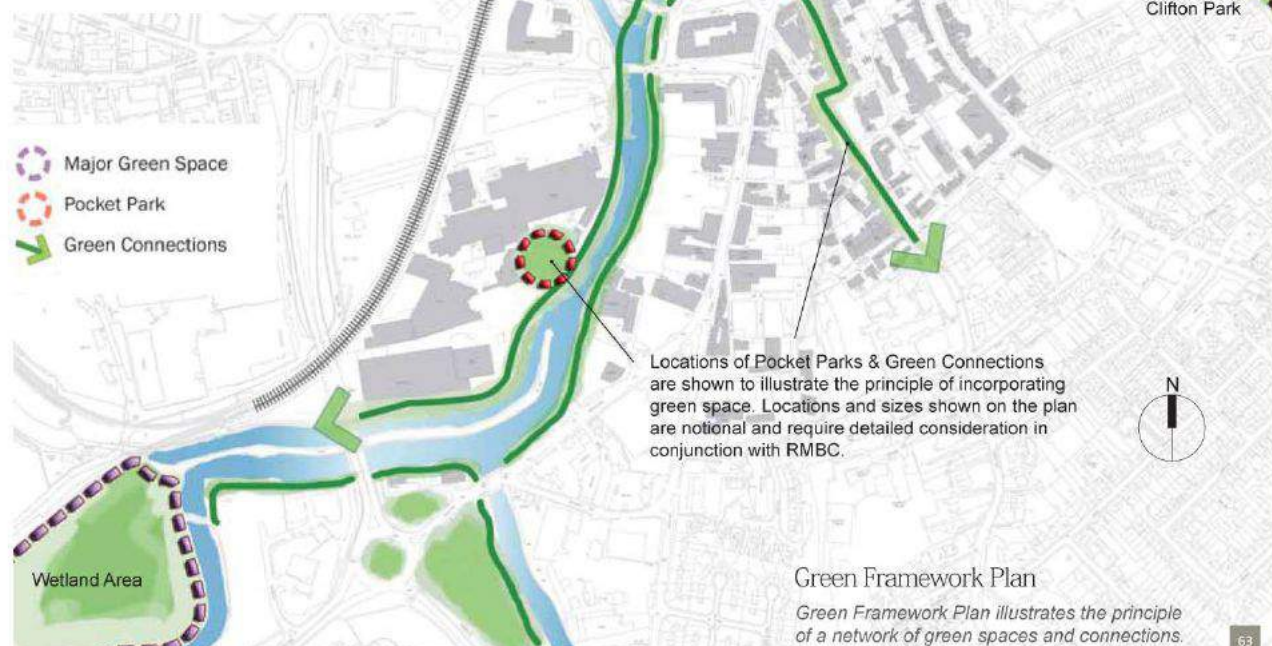
The introduction of a series of high profile new and upgraded green spaces. Designed to create informal relaxing places in the heart of the Town Centre.

Pocket Parks

A network of small scale green spaces and courtyards associated with new development. These should support the Major Green Spaces in providing new function within the Town Centre.

Green Connections

The 'Green Connections' strategy aims to connect the various green spaces in and around the Town Centre with a network of green routes. These may include tree lined streets, green visual links, planters, views towards countryside and more innovative solutions to 'green' Rotherham's streets such as green walls, roof gardens and sculptural features. The route between the retail core and Clifton Park should be strengthened with a green emphasis to highlight links to the park. Green links should encourage walking and cycling to green spaces to reduce car use and bring increased footfall to Town Centre businesses.



Green Framework Plan

Green Framework Plan illustrates the principle of a network of green spaces and connections.



Major Green Spaces

What are major green spaces?

The Strategy has identified the need for improved major Green Spaces within the heart of the Town Centre

A 'major green space' (as defined within this strategy) is a high profile public green space.

Why have major green spaces?

- A network of high profile green places.
- A setting for investment and new development.
- Offers new uses within the Town Centre such as 'lunch spots' and play.
- Opportunity to incorporate 'green' into hard spaces.
- A showcase for Rotherham's 'green agenda'.
- A welcome contrast to the hard urban environment.
- A setting for events and high quality buildings.

What type of green space is proposed:

Soft Green Spaces

Principles:

- Predominantly soft (raised lawns, planters, trees).
- Focus on informal seating opportunities (sculptural seating, terraces).
- Showcase for planting design - seasonal displays.

Hard Green Spaces

Principles:

- Paved spaces with a green emphasis (trees in grates or moveable planters, green walls or features).
- Flexibility between day to day 'passive' use and occasional events.

Soft Green Spaces



Cathedral Gardens, Manchester.



Union Square, San Francisco.

Hard Green Spaces



Millennium Square, Leeds.





A distinctive vision for Rotherham's major Green Spaces:

'Contemplative Garden'

A Vision for All Saints's Minster Yard

The aspiration for All Saints's Minster Yard is to provide a 'Soft' Green Space that reflects the historic setting of the All Saints' Minster, and provides informal seating for people visiting and those who live and work in the town. A magical lighting scheme could be incorporated within the existing trees and opportunities for local artists to exhibit (refer to Heritage Quarter p87 Zone of Influence for further detail).

'Contemporary Retail Retreat'

A Vision for Effingham Square

It is envisaged that Effingham square would be a 'Hard' Green Space, a contemporary flexible urban space that acts as a gateway into town and retreat from the retail streets. Effingham Square is a destination along the retail street loop, and as such it is important that seating be provided. The clock tower is a prominent feature within the existing space and needs to be incorporated into future proposals for the space (refer to Retail Core Zone of Influence p81 for further detail).

'Nature - Green Lung'

A Vision for the Wetland

An area adjacent to Bradmarsh Business Park has been set aside for use as a wetland area. This space will include trails and boardwalks and provide passive recreation for local people. There should also be a focus on providing for nature and biodiversity with educational and historical emphasis (refer to Baseline Section p10 for description of current proposals).

'Recreation - Town Scale Park'

A Vision for Clifton Park

Further active and passive recreation space should be provided in Clifton Park with a focus on heritage and the award winning Clifton museum. The park is ideal for large scale events and could also provide education of nature and biodiversity for local children. Funding opportunities and proposals for the rejuvenation of Clifton Park are currently being explored.

Existing:



Existing green spaces.

Inspiration:



A vision for All Saints' Minster Yard.

A vision for Effingham Square.



Pocket Parks

What are pocket parks?

Pocket parks are often relatively small spaces which are accessible to the general public (although are sometimes in private ownership). They are an invaluable means of distributing a series of usable, attractive and cared-for green spaces throughout an urban centre. They need to be accessible and easily found. There are many examples of successful pocket parks, especially in cities such as London and New York, where people are able step away from hectic surroundings and enjoy a quiet moment outdoors.

Why have pocket parks?

- To increase amount of green space in an urban centre and promote environmentally friendly practice.
- To distribute green spaces throughout a centre.
- To create a network of useful, user-friendly green spaces which people enjoy visiting on a regular basis.
- To give people pleasant, easily accessible places to sit without having to walk/travel a great distance from their base location.
- A showcase for new development and investment.
- To give people glimpses of greenery when walking/travelling about the Town Centre.
- To enhance the sense of pride in a place.
- To increase amount of seating/sitting places in Town Centre.
- To enhance the view from buildings which overlook the pocket park.

How are pocket parks different from major green spaces?

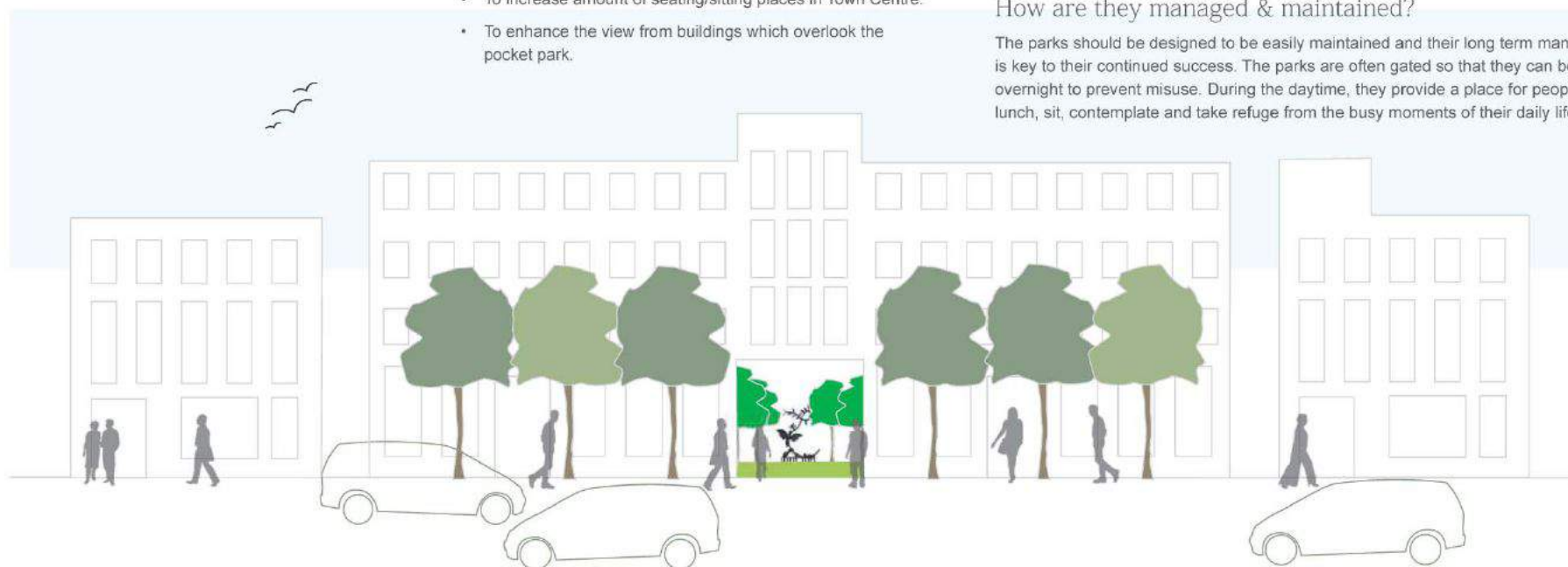
Within the hierarchy of the Green Framework, pocket parks are a secondary network of spaces both in terms of scale but more importantly profile. They should form part of the urban grain rather than being major destinations and are ideally situated on/very near to the street in order that they interact successfully with their surroundings and are well-used by local people.

Pocket parks are generally associated with new development or instead are positive solutions to existing leftover spaces within the urban fabric. They have buildings to one or more sides of them, which generates natural surveillance and enhances both the safety of the space and the feeling of safety for users of the park.

For pocket parks to be at their most successful, they should also have good visibility both into and out of them. This enhances their safety and security which prevents vandalism. Placing a focal point, such as a fountain, art installation or piece of sculpture, so that it is visible from the street, will help to draw people into the park and encourage repeat visits.

How are they managed & maintained?

The parks should be designed to be easily maintained and their long term management is key to their continued success. The parks are often gated so that they can be locked overnight to prevent misuse. During the daytime, they provide a place for people to eat lunch, sit, contemplate and take refuge from the busy moments of their daily life.



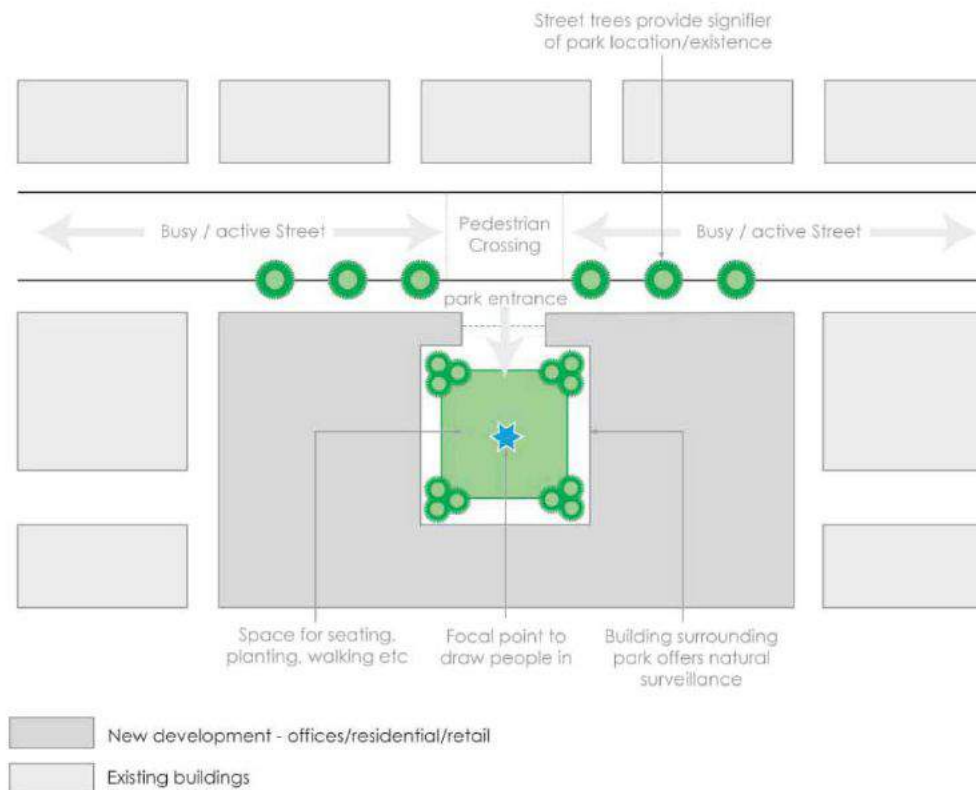


How should this relate to Rotherham?

The Green Framework plan proposes a network of connecting pocket parks to be delivered throughout the town in association with new development in locations such as Guest & Chrimes, Forge Island and areas adjacent to Clifton Park. Pocket parks can vary in shape and scale, but to be in keeping with the scale of Rotherham these parks should be up to 1,500 m² in size. The location of proposed pocket parks within the town is guided by the Green Framework Plan and their importance in providing green linkages to Clifton Park and other major green spaces. In addition to proposing pocket parks as an integral part of Rotherham's urban grain at ground level, this concept can be expanded to encourage both private and public green roofs. Such initiatives demonstrate Rotherham's green aspirations and are vital to health and well-being for the community and the environment.

Key principles for a successful pocket park:

- Ability to see out and in the space - no hidden nooks or crannies.
- Focal points to entice people in.
- Attractive & convenient design which welcomes people in.
- Easy to use - parks should be of a friendly scale.
- Safety & security is paramount - design, layout and adjacent use should create safe environment (people attract people to a place).
- There should be plenty of choices for seating.
- They should be surrounded and overlooked by buildings/activity on at least one side.
- They should have features which distract from the road/road noise.
- They can serve a variety of functions influenced by their design and surrounding building use.
- Clearly defined Management and Maintenance plan - often associated with adjacent development.





Case Studies:

Paley Park, New York

Paley Park in New York was completed in 1967 and is a celebrated example of a successful pocket park. It is a small space measuring only 390m² but it proves that even a very small space can be a vibrant meeting place as well as an area of relaxation and calm.

The park is located between two buildings and is positioned directly off the street. A waterfall situated at the back of the park creates a dramatic focal point as well as blocking out the noise of the surrounding city. There are moveable tables and chairs allowing people to be comfortable, and giving them control over where they choose to sit. The walls are covered with thick ivy, enhancing the sense of green, with seventeen honey locust trees providing dappled light and shade on a sunny day.

The park has gates which are locked at night to prevent misuse. It has a small refreshment stand which serves drinks and hotdogs. This provides both a reason for people to visit the park and a sense of natural surveillance as the park is always occupied by somebody.

Trevelyan Square, Leeds

Trevelyan Square is situated off Boar Lane, a major road running through Leeds city centre, enclosed by new development. It is about 1,300 m².

It is hidden from the main road but overlooked by the surrounding buildings, all of which open on to the square. The park also acts as an outdoor foyer to a hotel which fronts onto the square.

It is a place where many workers choose to have their lunch, sit and chat or have a moment to themselves. It has a convenient location amongst the offices and shops situated in Leeds city centre. It is one of very few such spaces and is therefore well-used all year round. It offers people a chance to interact with nature, experience changing seasons and enjoy sitting outdoors.

Pleasure Garden, Bonnington Square, London

Bonnington Square is a small community garden located near to The Oval in south London. It is an area of peace and calm and trust amongst a busy city filled with movement, and noise and pollution and rushing and stress. It is a much smaller example of a pocket park, but emphasises the value such spaces can achieve.

The space which the park occupies is the result of World War Two bomb damage. During the 1970s, the council installed children's play equipment such as some swings and a slide, but the space was not properly maintained and the site fell into disrepair. During the 1990s, when the council realised they owned valuable 'idle' land, and the local residents realised the space could be sold to developers, the Bonnington Square Garden Association was formed.

Local residents designed the space, by committee, along with the help of an architectural firm. The design was intended to create a gentle and beautiful slice of nature that could serve all of the community. It therefore has wooden swing and benches, small winding paths through planted areas and open areas of grass. The garden is never locked and is open to everybody. It is lit up at night and is maintained exclusively by residents who often have community 'help in the garden' days to keep it looking its best.



Paley Park, New York - A gap site park



Trevelyan Square, Leeds - formed by new development



Pleasure Garden, Bonnington Square, London - Community led garden as part of residential area.





Green Connections

Green connections can become a whimsical, innovative public 'eco art' feature in Rotherham Town Centre. The strategy proposes a design/branding competition to arrive at a final form for this concept but the illustrations shown below are intended to demonstrate the 'Green Connections' intent using bright coloured cubes and green 'living' cubes to create imaginative green features. The coloured cubes appear in a number of design propositions throughout this document to explore the Green Connections idea further, without being prescriptive on their eventual form, colour or material.

The Green Connections role is to link the network of major green spaces and pocket parks and their elements can jump into the green spaces themselves. In addition to being public art they can perform a number of functions such as signage, seating, lighting and greening. Looking beyond the immediate Town Centre, the green connection cubes could be an effective visual tool to highlight key pedestrian/cycle links and trails such as the route to and from Clifton Park, Boston Castle and the Guest & Chrimes site.





Banners with a green twist (living banners or solar panels)

Green space & Sculpture

'Extensive' Green roof (no public access)

'Intensive' Green roof / terrace (public access)



Information signage

Green connections linking green spaces

Street Trees to emphasise key routes





vision - rotherham lighting experience

The creation of the Rotherham Lighting Experience to promote the Town Centre as a destination

The Rotherham Lighting Experience would bring the evenings to life and become an ever changing reason to visit the Town Centre. A celebration of Rotherham and its residents that promotes a new incentive to explore. The strategy aims to provide:

- Permanent high profile installations and places.
- Temporary art locations which are constantly in flux from large scale installations to small scale surprises (interactive experience).
- An all year programme of events showcasing the Lighting Experience culminating in a Christmas light extravaganza.
- A showcase for local artists.
- An opportunity to showcase sustainability using low energy lighting such as LEDS and where possible should be self powered (for example solar powered)
- Enhanced lighting will have the further effect of improving town centre safety and security

In addition to creating a cultural showcase for the Town Centre, the concept aims to encourage investment, and opportunity for extended retail hours, tourism and to transform the community's negative perception of their Town Centre after dark. These benefits bring cost implications and a long term commitment in terms of both capital cost and maintenance cost is required.

The Rotherham Lighting Experience theme overarches and complements the 'everyday' lighting proposed for the Town Centre. Refer to the Public Realm Palette (p43) and the Design Guidelines Section (p107) for details of light fixtures, building mounted lights and accent lighting.



A website leaflet could market the Rotherham Lighting Experience.



Lighting Proposals

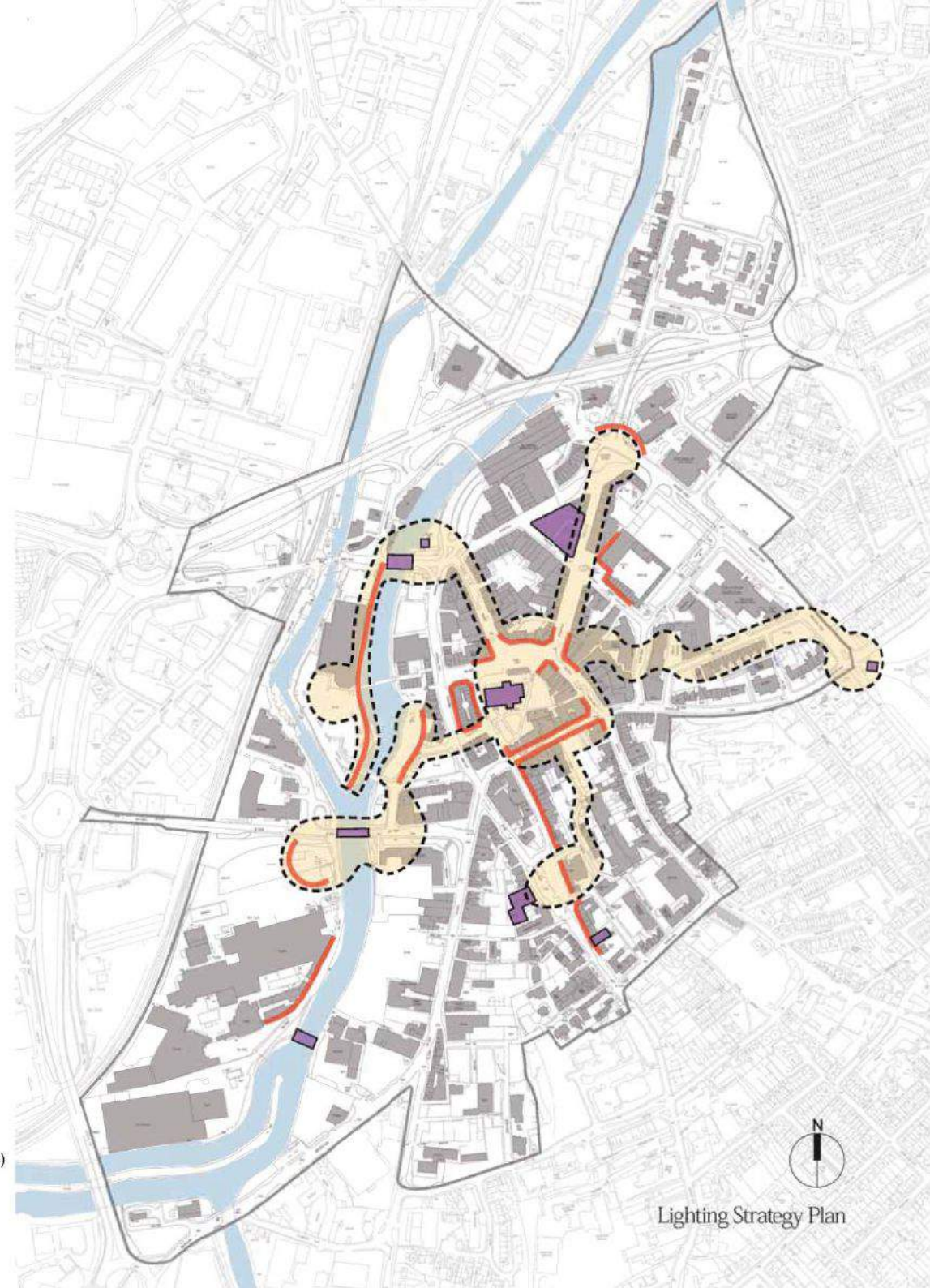
The proposals plan (see right) provides a focus for permanent and temporary lighting, highlighting key landmarks (such as All Saints' Minster and the Weir), key frontages (such as the Forge Island riverside and High Street) and key places (such as Effingham Square and Westgate Demonstrator).

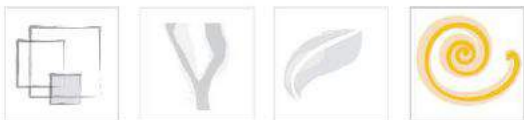
Flexible and adequate electricity/lighting infrastructure is key to fulfilling this vision and public realm improvements should take account of the potential needs as part of their design. Features such as additional power points could become a necessary requirement of improvements within the areas shown. Where possible installations should be self powered.

A number of iconic 'Rotherham Lighting Experience' ideas have emerged through this strategy including:

- Forge Island Ribbon of Light.
- The River Don and wall - art canvas.
- A 'magical' All Saint's Minster Yard.

-  **Landmark Buildings/Features**
(Landmark Lighting)
-  **Key Building Runs/Frontages**
(Facade Lighting)
-  **Key Places**
(Lighting Routes & Destinations)





Inspiration

Case Studies

Michigan Avenue, Chicago - Magnificent Mile Lights Festival (www.themagnificentmile.com).

An annual festival which brings Chicago's premier shopping street to life in the evening. Attractions include art installations, sculptures and fireworks.

Turin Light Festival - Each year between November and January the streets and squares are illuminated with temporary light installations by specially commissioned artists.

Professional Lighting Designers' Association (PLD) Workshops (www.pld-a.org/180.0.html).

The Professional Lighting Designers' Association hold workshops throughout the world to create temporary lighting installations. Birmingham held an event in 2006 where international profile artists worked with local students to create a number of installations throughout the city.

Build on Current Success -

- Significant progress has been made to bring people back to Rotherham Town Centre as part of a wider cultural agenda. The Rotherham Open Arts Festival 2007 was held in the heart of the Town Centre at All Saints' Square in the famous Spiegletent during 10 days in September. The event included 32 events offering a wide offer including a particular emphasis this year on local artists and attracting a wide audience including families and young people.
- The Rotherham Lighting Experience idea can build upon the successes and widen the appeal of existing initiatives and events such as The Open Arts Festival and the Rotherham Show.
- RMBC are currently developing a number of exciting light/art projects with high profile artists.

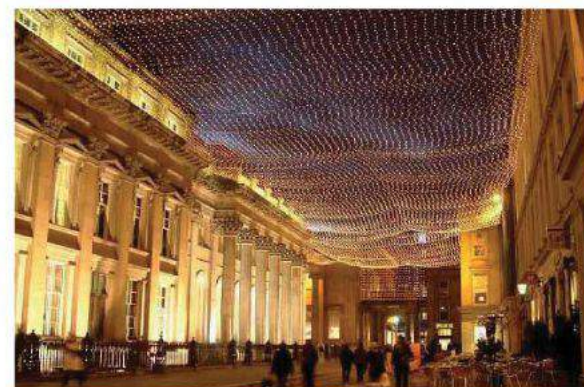


Lighting underpasses create safer and more attractive environments for those passing through.

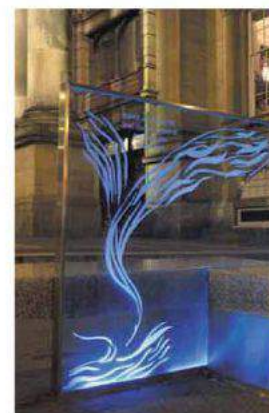
Light projections, either onto buildings or the floor, create artistic features which can change throughout the year.



Floor lighting can help direct people to areas of interest, whilst creating an attractive night scene.



The net lighting at Glasgow creates a sparkling roof to a public space.



Sculptural features can incorporate lighting in dynamic ways.

public realm framework

- Spatial Hierarchy
- Zones of Influence
 - Retail Core
 - Heritage Quarter
 - Eastern Edge
 - Western Edge (Riverside)

framework - spatial hierarchy

A Spatial Hierarchy that provides a framework for harmonious public realm improvements

The objective of the Spatial Hierarchy is to provide a working tool-kit to inform decisions on future public realm improvements in Rotherham Town Centre. A positive challenge facing Rotherham is the level of proposed redevelopment planned within the Town Centre and therefore it is of great importance that this strategy forms the starting point for planning future schemes. This approach creates a methodology that can evolve and adapt with the Town Centre.

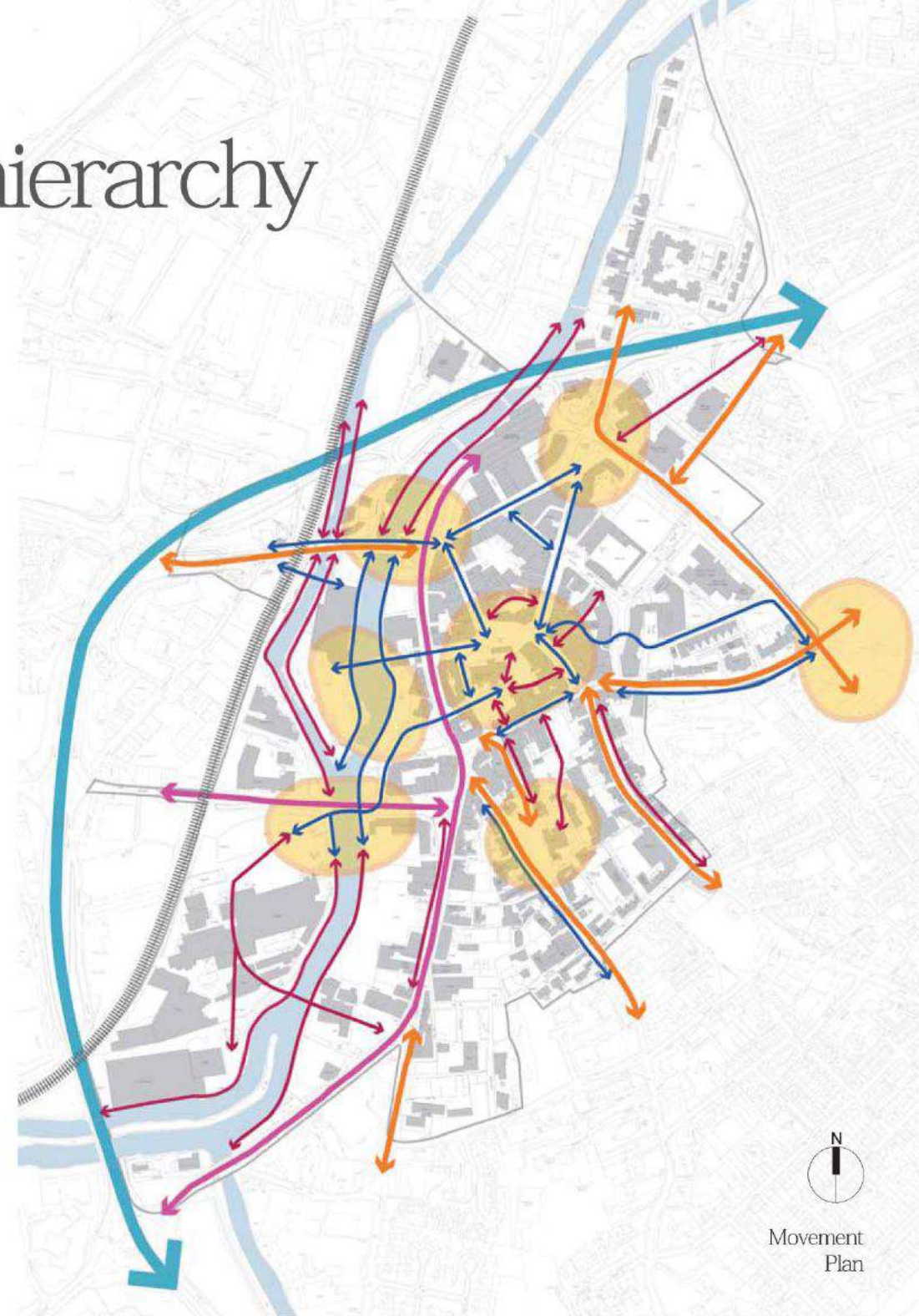
Movement Plan

The following plan considers the changing movement patterns in relation to a number of emerging development site proposals. As the Town Centre develops, new places and destinations will emerge and it is important that these are weaved into the existing network.

It is recommended that pedestrian and cycle movement should be increased in priority within the Town Centre and therefore in time, vehicular dominance of the environment becomes reduced. In tandem Rotherham is continuing to improve its public transport offer including the emerging proposals to upgrade the train station. RMBC's Cycling Strategy sets goals to significantly raise the profile and cycle usage within the town and recommends a wide variety of initiatives. These include providing user friendly, efficient, safe cycle routes and parking, which link in with public transport, key destinations and new development. It is crucial to the success of the Town Centre that multi-modal accessibility is available and the quality of the experience is improved to encourage wider audiences to visit.

The movement plan has been developed to inform the spatial hierarchy plan, shown on the following page.

-  Major Highway Infrastructure
-  Primary Vehicular Route
(with cycle & pedestrian provision)
-  Secondary Vehicular Route
(with cycle & pedestrian provision)
-  Primary Pedestrian Route
(with cycle provision outside of fully pedestrianised areas)
-  Secondary Pedestrian Route
(with cycle provision outside of fully pedestrianised areas)
-  Key Places



Spatial Hierarchy Plan

The following plan establishes a **Spatial Hierarchy** for Rotherham Town Centre. This sets the councils expectations for **investment** and **design approach** for both existing and future Streets, Spaces, Links and Gateways.

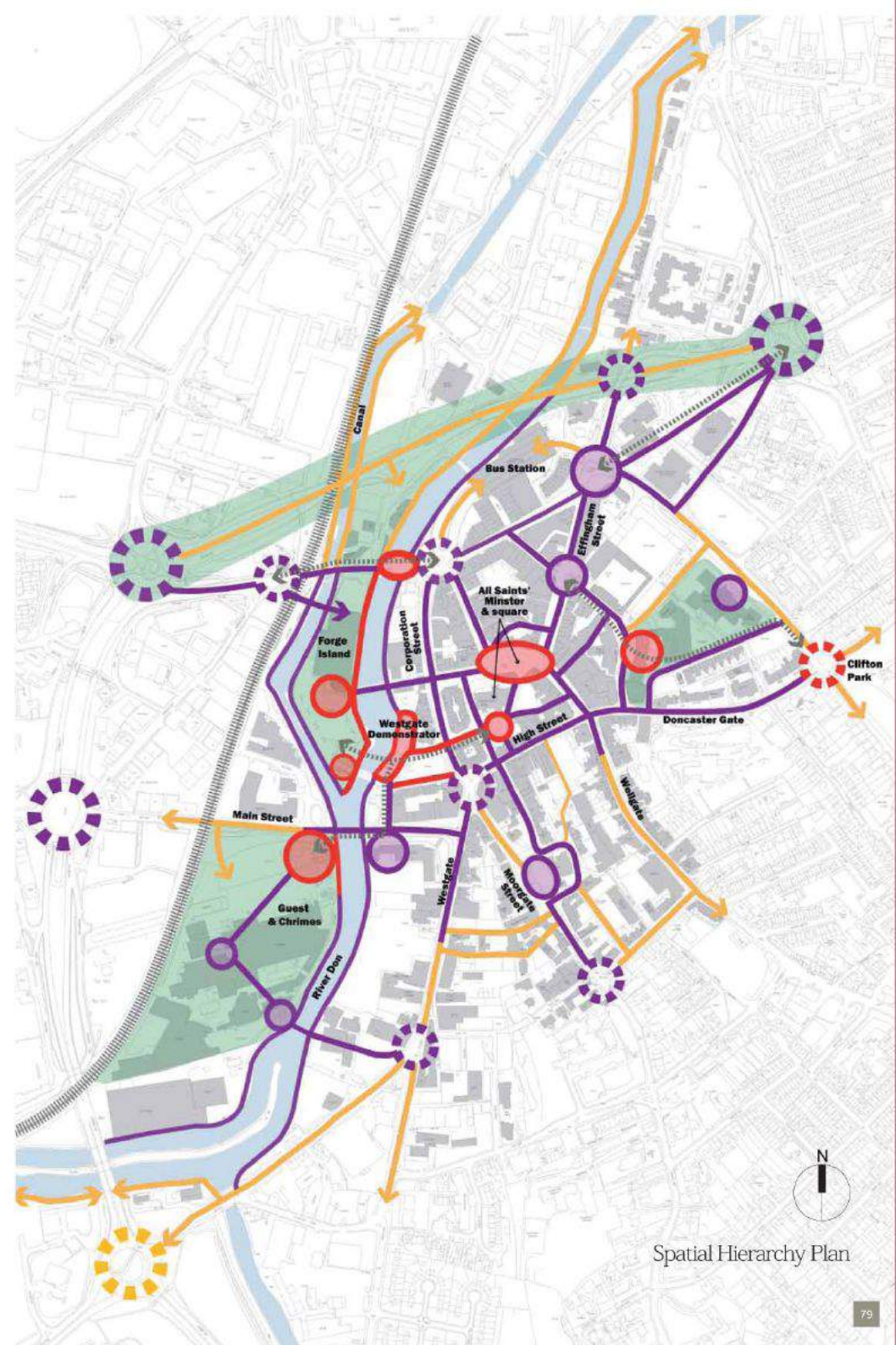
Three levels of investment are proposed for improvements (**Premium, High and Standard Quality**). As schemes are developed this plan should become a starting point to the design and planning process. Once the level of investment is established, design guidance is given in the Public Realm Palette (refer to p43). For example if a scheme is proposed for Corporation Street the use of a High Quality Public Realm Palette is applicable. The designer/developer/stakeholder can refer to p46/47 for further details on the high quality design approach.



Design concept diagram for high quality palette (p46/47)

As with the Movement Plan (see left), the Spatial Hierarchy Plan considers that large areas of the Town Centre are likely to change through new development. In such areas the eventual street pattern will be designed in detail and in conjunction with RMBC. These areas are therefore defined on the plan as '**Areas of undefined future change**'. Within these locations the plan intends to set broad principles to guide future projects and is not intended constrain proposals. For example Forge Island is intended to deliver a premium quality riverside walk and incorporate a series of public spaces within the site, which pick up pedestrian desire lines.

A number of **key links** into the Retail/Heritage core of the Town Centre have been identified, which should be a particular focus for investment. Ideas for these links are given throughout the strategy.



framework - zones of influence

The creation of Zones of Influence set a design tone for Public Realm in distinctive areas

Zones of Influence Plan

The following zones plan shows four **zones of influence**, which set a design tone for the public realm approach in each. This is intended as an overlay to the **spatial hierarchy** plan and aims to encourage a harmonious yet locally responsive public realm strategy. This section of the report provides propositions and design guidance to illustrate how the strategy could be applied to specific places. In addition it suggests variations to the Rotherham Town Centre Public Realm Palette to bring out local distinctiveness.

The zones of influence are specifically chosen with public realm improvements in mind. The following pages provide an overview, a vision, inspiration and project propositions for each.



Retail Core



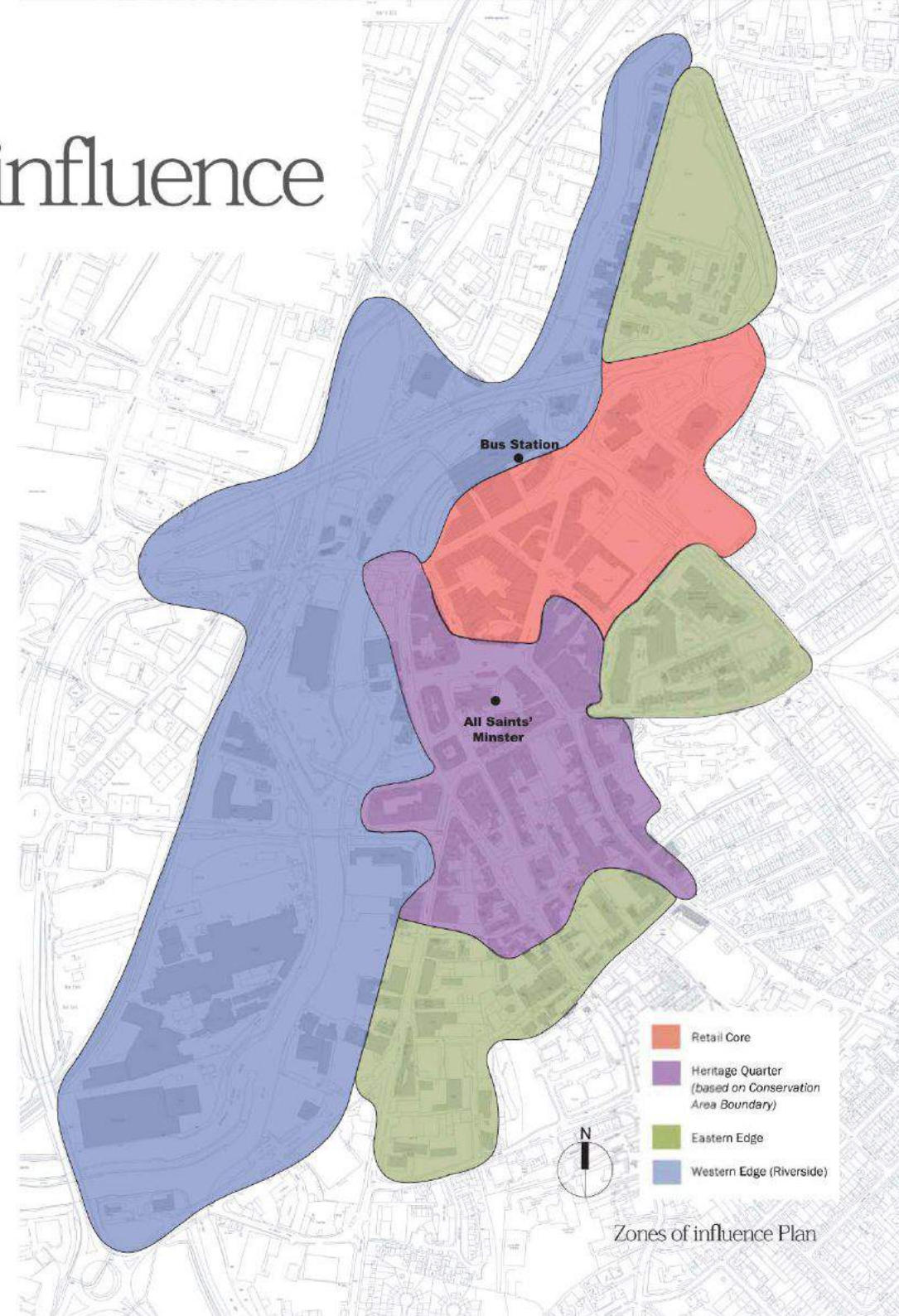
Heritage Quarter



Eastern Edge



Western Edge





retail core

Retail Core

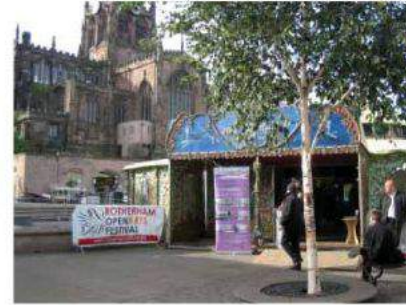
Overview

- Predominantly pedestrian network of streets with much of the historic street form still in place. The townscape/urban form is likely to remain intact in the future.
- Retail has struggled over recent years but the market is a successful pedestrian generator. All Saints' Square creates a high quality focal point to the pedestrian core and is the heart of Rotherham's existing public realm.
- Buildings are generally high quality, however ground floor shop fronts are often poor.
- Public realm is fragmented and the retail core is a tapestry of various public realm schemes and materials.
- Existing Council Buildings located north of Drummond Street are planned to be relocated.
- Environment to the north of the area is dominated by highway infrastructure, 60's/70's architecture and benefits from steep topography with views over the Town Centre and surrounding countryside (loss of historic pattern).

Existing Images



Old Town Hall, which is a high quality landmark building. The indoor arcade offers variety in the retail core.



All Saints' Square: A setting for performance, events and markets.



Farmers market on Effingham Street.



Above: College Street building facades.

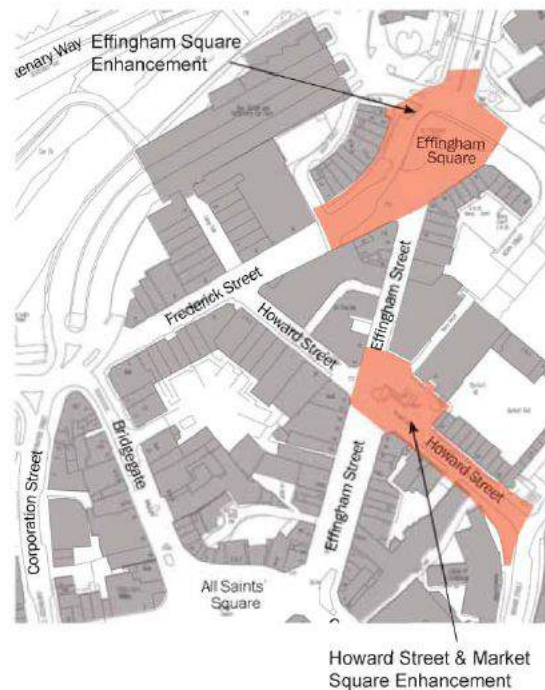


Above: Effingham Street hosting the temporary outdoor market.

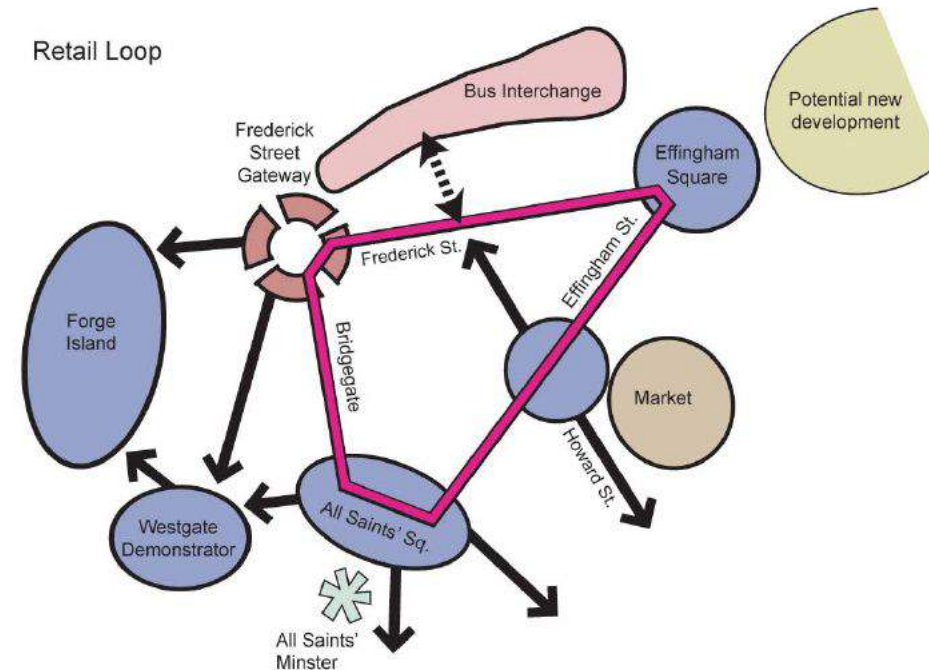


Vision and Inspiration

- A network of high profile vibrant public spaces, streets and links as a setting to re-establish Rotherham as a shopping destination (create a retail loop).
- Primary purpose is to attract retailers and shoppers through creation of a clutter free, accessible, flexible, public realm that puts pedestrians first. A retail friendly environment (pedestrian desire lines, maximise footfall, contemporary design to attract retailers and shoppers).
- Street trees as a integral part of the streetscape to create a healthy, attractive Town Centre.
- Shelter and canopies to enhance the shopping experience and create comfort through all seasons. Opportunities should be explored along main retail streets and pedestrian nodes (such as Effingham Street and Frederick Street).
- Emphasise views and links with All Saints' Minster.
- Opportunity to incorporate temporary or permanent market stalls into the streetscape within the retail core.
- The former civic campus to become a positive piece of urban townscape.



Retail Loop



Inspiration Images



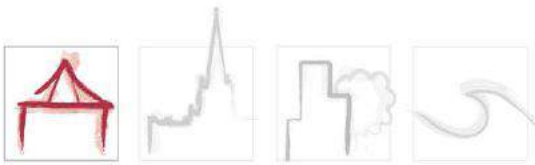
Clutter free 'retail carpet'.



Canopied, sheltered pedestrian street



The following pages set out propositions and guidance for the retail core.



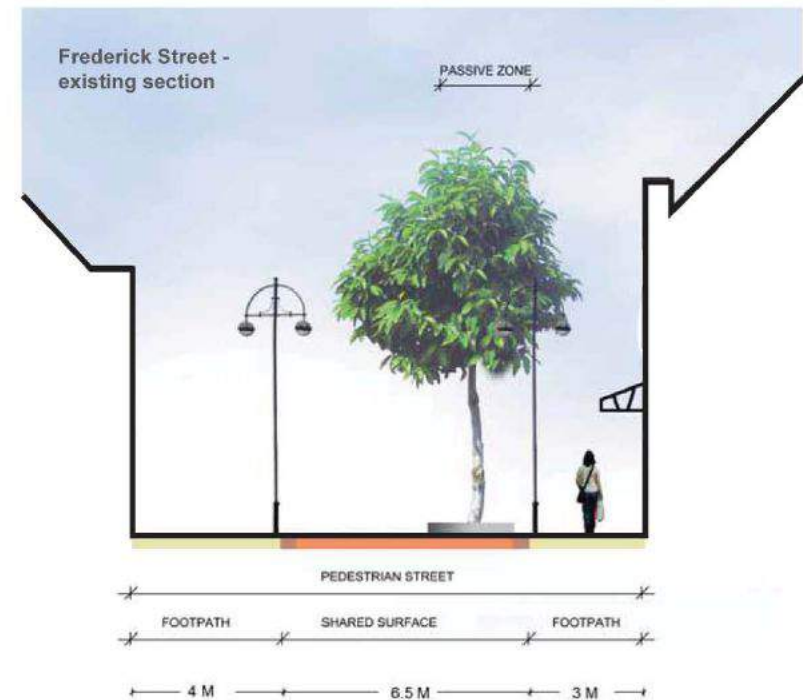
The following sections illustrate how the retail loop streets could be improved and reorganised.

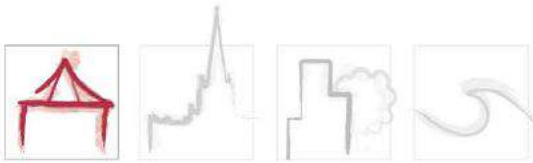
Typical Street Sections for Frederick Street

- Create a contemporary retail carpet with an option to incorporate street trees and canopies to provide a sheltered shopping experience.



Frederick Street.



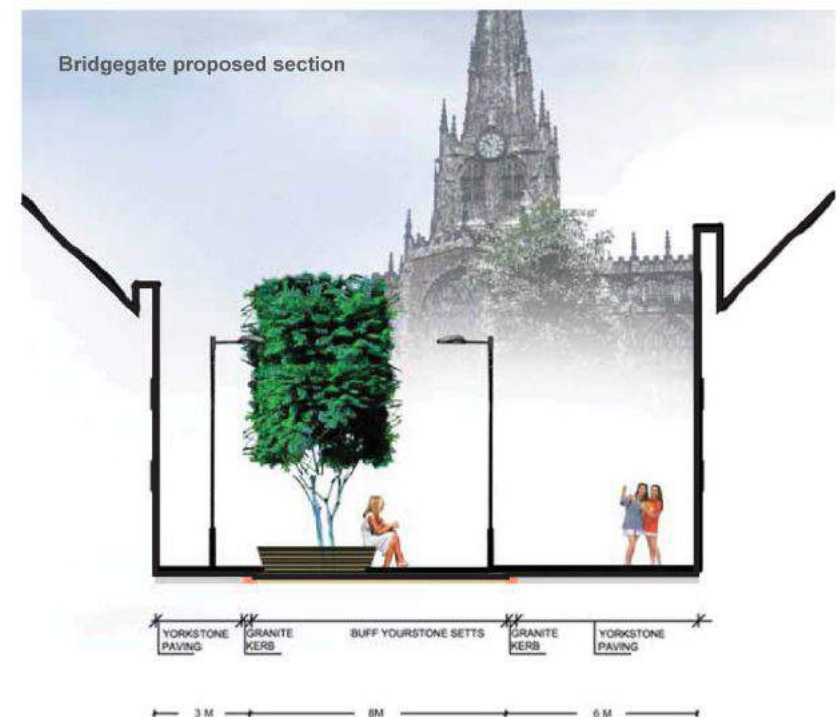
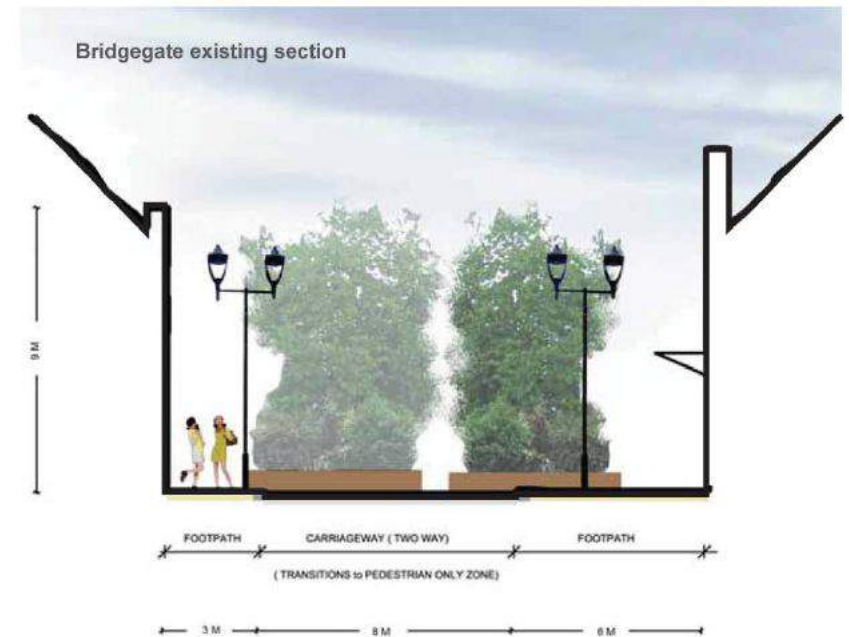


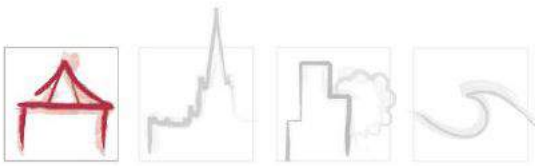
Typical Street Sections for Bridgegate

- Create a shared surface environment and frame view of Minster.
- Incorporate informal seating into planters, remove clutter and frame visual link with Minster.



Bridgegate is an important pedestrian link to the historic core of the Town Centre.





Effingham Square Enhancement

The potential redevelopment adjacent to Effingham Square offers an opportunity to enhance the Square as part of the 'retail loop'. Within the vision Effingham Square is proposed as a major green space 'a contemporary retail retreat'. The Spatial Hierarchy proposes that the square use the High Quality Palette of surface materials.

The following design principles are proposed for Effingham Square:

- Predominantly hard surfaced with strong grid of trees to provide a landscape framework to the space.
- Design should accommodate opportunities for events or markets, therefore trees could be planted in moveable containers to create a flexible green space.
- Provide a variety of seating opportunities including shade and shelter for comfort.
- Maintain desire lines and visual links for all key pedestrian routes, emphasising movement along the retail loop and across the new supermarket site.
- Incorporate 'Green Connections' (whimsical, innovative public art features such as green walls or green cubes that link the network of major green spaces and pocket parks throughout the town, p69).
- Clock tower should be reinstated into the square retaining its important historic focus.
- Opportunity to rationalise or relocate taxi rank to increase pedestrian space.
- Space should interact with the adjacent new development, with safe, direct pedestrian access provided to it across Drummond Street. A pedestrian dominant environment is a key principle, however the extent of this approach is dependent on the outcome of future plans for downgrading/humanising Centenary Way.
- The public space would benefit from downgrading Drummond Street. However, in response to the busy road a feature should be incorporated into the square to buffer pedestrians.

- Existing active frontage to be retained and new development should provide high quality frontages addressing the space. The poor quality frontage adjacent to the taxi rank requires improvement and provides a long term opportunity for redevelopment.

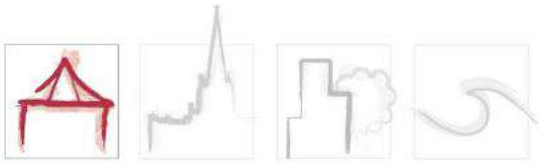
Howard Street Market Square Enhancement

The proposals offer an opportunity to upgrade and rejuvenate the 'retail loop streets', Howard Street and Market Square. The Market Square junction is currently a bustling meeting place, however the dated water feature and mobile catering vans overpower the space, creating a cluttered area which acts as a barrier to movement. The creation of a new public/events space would open up the space and allow a new market entrance. The Spatial Hierarchy proposes a High Quality treatment.

The following design principles are proposed for Howard Street Market Square:

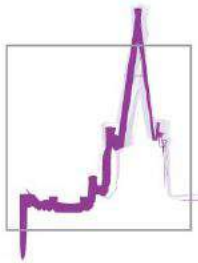
- Provide a welcoming 'entry mat' for the indoor market.
- Upgrade the existing market entrance creating a landmark feature such as a canopy.
- Maintain existing active frontages to support the space.
- Tie in with Green Connections vision (refer to p69).
- Design should allow flexibility to accommodate market stalls.
- Tie in with proposed RCAT premium quality space associated with it's proposed redevelopment.





A vision for Effingham Square.





heritage quarter

THI (Townscape Heritage Initiative)

Overview

- Rotherham's Heritage core which retains much of the historic street pattern.
- All Saints' Minster is located in the centre of the THI area and is the Town Centre's main landmark, creating a visual point of reference. The Minster Yard is the only green space located in the heart of the Town Centre.
- The Minster Yard is planned as a first phase of public realm improvements to follow a number of adjacent projects including the Living Over The Shops (LOTS) initiative on Vicarage Lane and the redevelopment/refurbishment of a number of key blocks along Church Street.
- All Saints' Minster Yard currently does not function as a destination and suffers from anti-social behaviour.
- High street suffers from some poor quality built form and has a number of blank facades.
- A network of medieval ginnels, lanes and yards exist within and on the edges of the THI area. These add richness and an extra layer of tight grained links and spaces, however they are generally in poor condition and can encourage anti social use. The recent addition of residential use and improved lighting along Vicarage Lane has demonstrated how such routes can be reinvigorated.

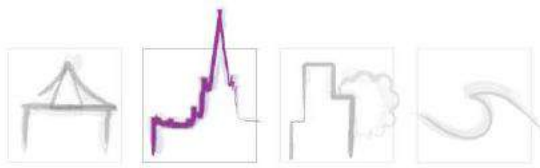
Existing Images



All Saints' Minster Yard.



Above: High Street.



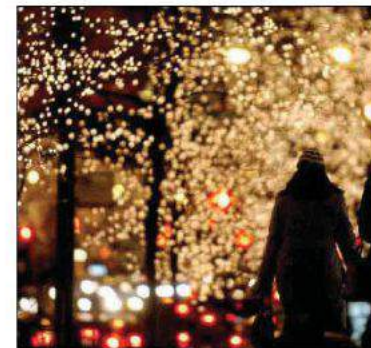
Vision and Inspiration

- To improve the current unco-ordinated public realm and to create a high quality loop around the Minster.
- Minster Yard is to become a contemplative urban garden (at the heart of the proposed green framework).
- High Street is to be rediscovered as a destination with a dramatic public realm proposed to transform its decline creating a new purpose as a bustling cafe/restaurant street.
- To reinvigorate the historic ginnels, lanes and yards.

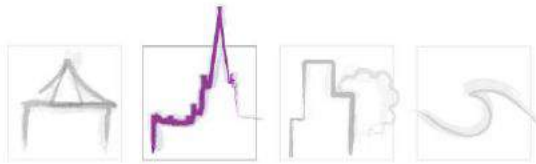


Public realm
improvements to THI
streets and spaces

Inspiration Images



The following pages set out propositions and guidance for the THI (Townscape Heritage Initiative) Area.



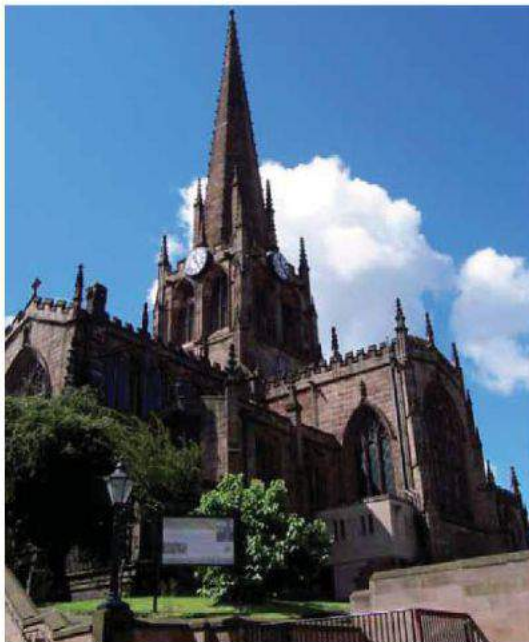
All Saints' Minster & Yard

All Saints' Minster Lighting

All Saints' Minster marks the heart of Rotherham Town Centre and is a landmark on the skyline from the majority of approaches into the Town Centre.

Its lighting should make a statement and can be the central part of the Rotherham Lighting Experience (refer to p73).

The illustration shows the Minster lit theatrically, which may be appropriate during festivals and events, however a more understated approach would be used at other times. In tandem with immersing the Minster Yard in fairy lights, the heart of the Town Centre can become a magical experience in the evenings.

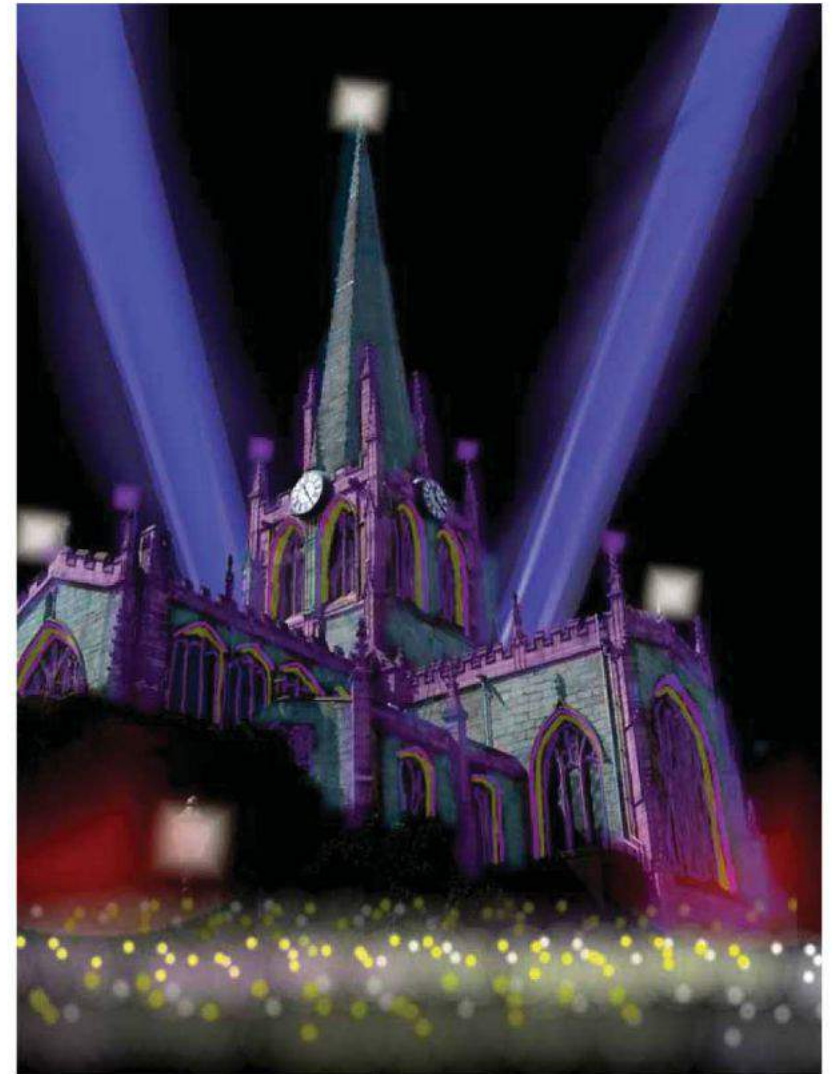


All Saints' Minster.

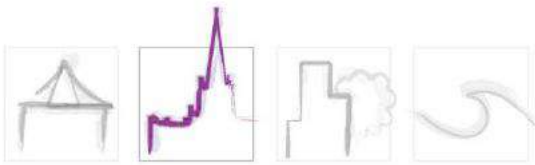
All Saints' Minster Yard

Create a contemplative urban garden, which becomes a destination. Design should maintain the traditional minster garden character with a modern twist:

- Understated public realm with high quality natural paving (retain existing) and durable timber seating. Seating should become a feature in the garden and be incorporated into wide sculptural stone kerbs/walls (incorporate skateboard deterrent measures into kerb).
- Additional paved space outside the Minster.
- Incorporate planting within the lawns - opportunity for seasonal displays - a reason to visit.
- Opportunity to introduce small scale sculptures (either incorporated into kerb feature or free standing).
- Take opportunities to create active frontages onto the garden to create positive surveillance and use. Where active frontages are not feasible, backs of buildings should be improved (e.g. facade improvements and enclosures for bins). The design of the Yard should keep safety in mind by minimising 'hidden places'.
- Magical lighting scheme for the yard, utilising feature lighting instead of reliance on street lights.
- Improve adjacent alley links to College Street, High Street and All Saints' Square to provide historic routes which are attractive and safe. Extend this initiative beyond the Minster to pick up College Lane, The Red Lion Yard (privately owned by Red Lion Yard Association) and Snail Hill.
- Retain majority of existing trees within the Minster Yard. Seating should offer opportunities to sit in the shade to shelter on sunny days. Tree canopies could be lit with fairy lights to create a feature.
- Emphasise entries from All Saints' Square and Church Street. Opportunity for bespoke entry feature in keeping with Heritage character.



Create a stage for All Saints' Minster.



Upper Millgate, Church Street and Market Street

Create a transition zone between the Westgate Demonstrator Project (modern, high impact design), All Saints' Square (contemporary public space) and All Saints' Minster and Yard (heritage):

- Simple, uncluttered, unifying scheme - focus on paving and street trees.
- A high quality harmonious setting for All Saints' Minster.
- Let the architecture and building frontages take centre stage.
- Bring some elements of Westgate and All Saints' Square into this character area.
- Treat as a link to spaces rather than a destination - simple carpet of natural stone slabs to draw attention to distinct destinations such as All Saints' Square and Minster Yard.
- Further introduce street trees into the area. Upper Millgate's existing spatial success is due to the framework created by trees. There is a strong rationale to introduce additional trees into this area to evoke a European style street setting for the minster, where trees frame views, define cafe spill out space and pedestrian movement space. The street trees are also intended to tie in with the Minster Yard which is currently the only significant greenspace in the town centre.
- Add and emphasise remnants of historic paving to highlight heritage richness.

Upper Millgate & Market Street:

- Create a tree lined character.
- Clearly define 'movement' and 'stopping' spaces and reduce clutter (Upper Millgate's width provides an opportunity).
- Retain existing yorkstone on Upper Millgate and replace remaining area with natural stone setts (layout of paving to create continuity with All Saints' Square).
- Replace paving on Market Street with yorkstone slabs.

Church Street:

- Retain existing yorkstone slabs and replace concrete cobbles with natural stone setts.

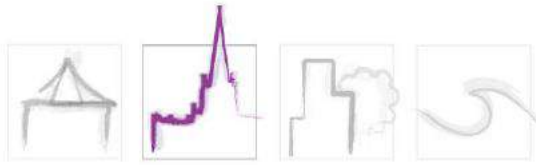
Existing Image



Upper Millgate

Inspiration Images





THI Streets - High Street

The High Street requires a drastic approach to transform its current decline and poor image. This offers a one off opportunity within Rotherham's existing townscape to create something very different in order to re-invent the High Street:

- Bold, high impact design - an arty, vibrant, quirky place.
- Arts and food focus with independent shops (associated with new urban living - LOTS Residential Development Schemes on upper storeys) - a reason to visit.
- Proposed yorkstone paving and furniture is intended to tie in with THI heritage character to create a cohesive loop around the Minster, however certain features should provide a bold new identity for High Street.
- Topography is an asset providing dramatic views and opportunities to emphasise existing and provide new focal points - a landmark sculpture/spire.
- Contemporary scheme focusing on lighting, sculptural features, paving with a twist, vibrant shop frontages and cafe spill out space.
- Improve alley links to and from the High Street including Snail Hill, which connects to the Crofts area.

Existing Image



Poor quality built form on High Street, shutters create an inactive street frontage



High Street



Alley off High Street leading to Snail Hill.

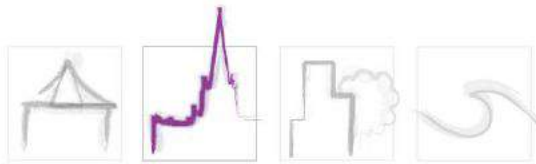
Inspiration Images



O'Connell Street, Dublin.
The Spire of Dublin



Exchange Square, Glasgow



The Crofts and Town Hall

Overview

- Moorgate Street is a major route into town for pedestrians and vehicles.
- A distinct area located on the periphery of the Town Centre including the Town Hall and a number of popular bars and pubs.
- Its townscape is rich in quality with a number of high quality buildings and churches. Off Moorgate Street the urban grain is fine with a number of winding routes into the Town Centre such as Snail Hill.

Existing Image



Town Hall plaza



Rebranding The Crofts

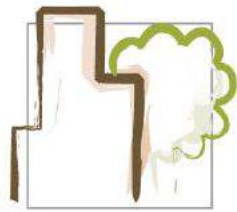
Vision & Inspiration

- The Crofts - a distinct, vibrant urban quarter to be celebrated - a 'branded' destination in its own right:
 - A branded suite of furnishings such as signage, shop fronts, information boards, banners.
 - A new festival or event associated with the Crofts.
 - Its quirky streets, alleys and architecture lends itself to introducing more independent shops and cafe culture.
- A close knit network of shared surface lanes, yards and ginnels. Transforming negative alleys and links into positive experiences and places for public art. Tie in with approach for wider Heritage Quarter.

Inspiration Images



Shop fronts and banners.



eastern edge

Effingham Street (North)

Overview

- Peripheral commercial use.
- Access to riverside along Effingham Street is blocked by commercial buildings.
- Fragmented in nature on approach to the main roundabout.
- Proposed Leisure Centre to utilise large site along Effingham Street (north) and St Ann's Road.
- Adjoins the Rawmarsh Parkgate and Rotherham East (HMRA) Housing Market Renewable Areas.

Existing Images



Effingham Street (north)

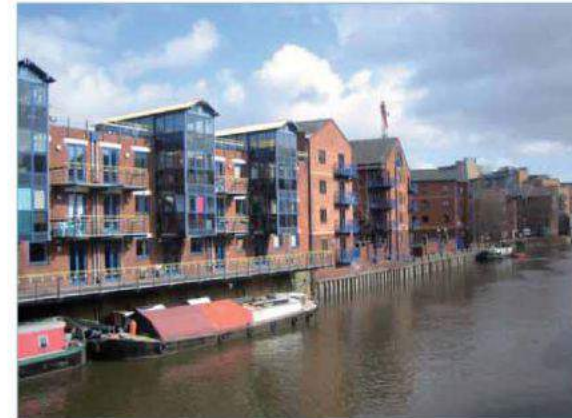
Vision & Inspiration

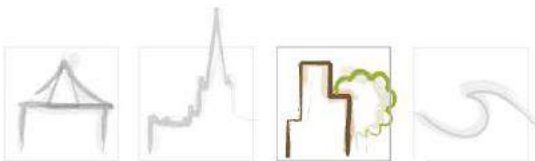
- Enhance physical and visual interaction with the River (currently no access). Seek opportunities to deliver a continuous riverside walk in the longer term.
- Improve pedestrian link to new Leisure Centre from Town Centre.
- Improve linear gateway into Town Centre.



Improved interaction with river

Inspiration Images





Clifton Park Link & RCAT

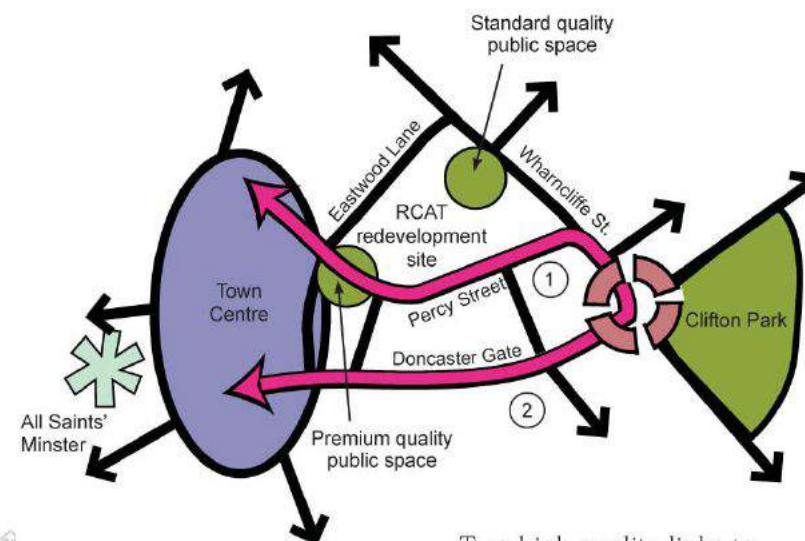
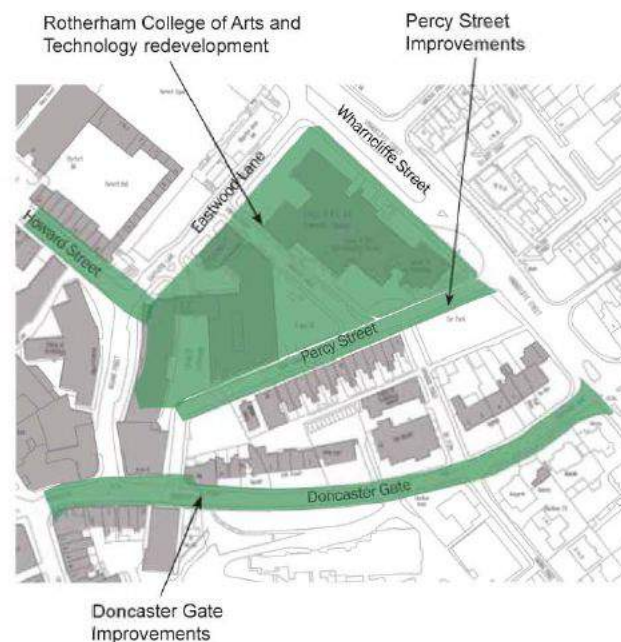
Overview

- Existing Rotherham College of Arts and Technology Site (Planned redevelopment of site by RCAT).
- Doncaster Gate is a leafy commercial/residential street linking the Town Centre with Clifton Park. Steep topography is an asset, however the park is out of view from within the Town Centre.
- Architecture is generally high quality Victorian, with low natural stone boundary walls.

Vision & Inspiration

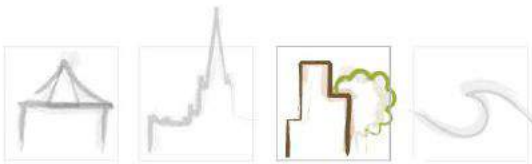
- Opportunity for a new RCAT campus to bridge the gap between Clifton Park and the retail core including the provision of new public spaces (pocket parks).
- Legible well connected layout creating a priority for pedestrian links into the Town Centre from adjacent residential areas.
- Strong urban form addressing Wharnclyffe Street and Drummond Street.
- Green focus for streets and development (bringing the park into Town).

Existing Image



Two high quality links to Clifton Park Link

The following page sets out propositions and guidance for the Clifton Park Link and RCAT Area.



1. High quality link along Percy Street

The planned redevelopment of the RCAT site elevates the importance of Percy Street as a high quality route connecting Clifton Park to the Town Centre. The route will pass through a new premium space that celebrates the college bringing it into the Town Centre.

2. A Strong green connection along Doncaster Gate

Doncaster Gate and Drummond Street should become part of the Clifton Park experience. The visual link from the Town Centre along Doncaster Gate and Drummond Street should be a positive advert for Clifton Park, which is only a short walk away. While the architecture along Doncaster Gate is generally high quality, the main junction is flanked by poor facades. Drummond Street has open views towards Clifton Park but would benefit from green enclosure to improve the microclimate whilst emphasising the green link. Pedestrian and cycle links should be encouraged between the Town Centre and Clifton Park to increase use and reduce reliance on cars.

Existing Images



Clifton Park gateway.



Doncaster Gate.



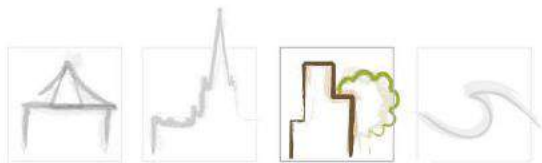
By applying the principles set out in the Green Connections (refer to p69), the visual link can be transformed with or without adjacent redevelopment.

What's going on in your town centre?



A network of 'message board' information signs could be situated in key locations throughout the Town Centre. This should aim to increase access to the Town Centre for all groups of the community. In addition to displaying useful information they can become features in their own right. This indicative design ties in with the Green Connections theme (refer to p69).

The example shown might be located at the bottom of High Street to advertise the close proximity of Clifton Park and what's on offer in the park.



Westgate & Moorgate

Overview

- Major vehicular routes into Town Centre along Westgate and Moorgate.
- Currently plays a support role rather than feeling part of the Town Centre. Predominantly residential/commercial/light industrial use.
- The wider Westgate area is planned as a future phase of redevelopment, incorporating riverside access.

Existing Images



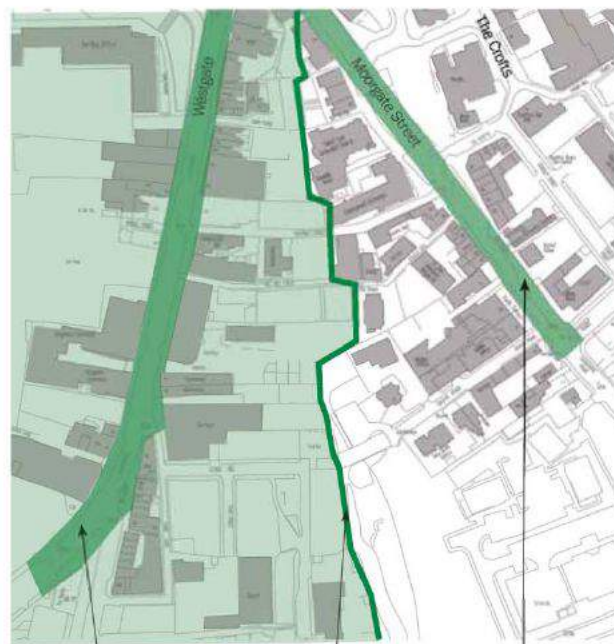
Westgate.



Green roof at Moorgate Crofts Business Centre.

Vision & Inspiration

- Westgate should be defined as a distinct quarter established as an accessible piece of Town Centre. The route along Westgate should announce arrival in the Town Centre.
- A Design Code is in place for the whole Westgate area that identifies four distinct character areas: Weirside, Westgate/ Main Street, Riverside and Hillside including mixed use, retail, commercial and residential.
- Its aims are to create a legible street hierarchy, utilise topography, reinforce and reinterpret existing character.



Improve Westgate corridor

Design Code area within Rotherham

Improve Moorgate corridor

Inspiration Images



(Roger Evans Associates September 2005, Design Code for the Rotherham Town Centre River Corridor).



western edge

Transport

Overview

- A major gateway into the Town Centre by car (Centenary Way & Multi-storey car park) train (railway station) and by bus (bus interchange).
- The Train Station is an important pedestrian/vehicular gateway which currently undersells Rotherham. There are plans to upgrade the train station and further expand the new Bus Interchange.
- Poor pedestrian environment.
- Adjoins the Rotherham West HMR (Housing Market Renewal Area).

Existing Images



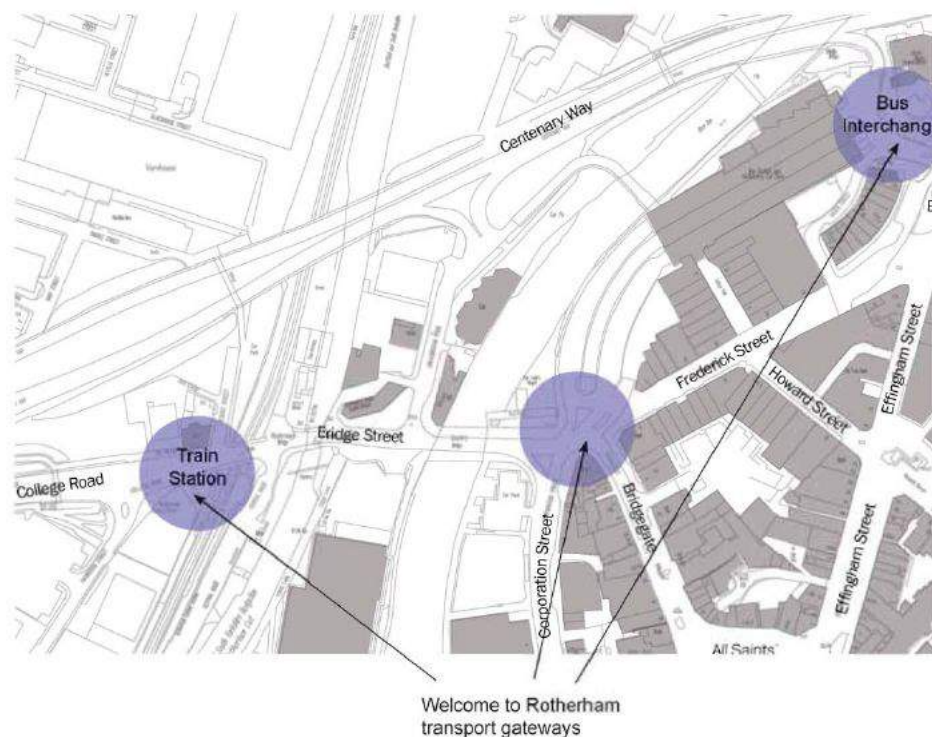
Train Station



Bus Interchange entrance - Vehicular dominant space.

Vision & Inspiration

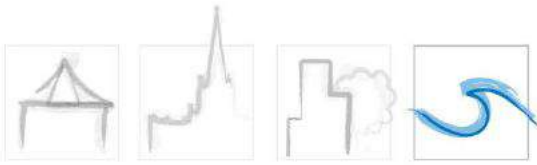
- A positive welcome to Rotherham Town Centre and a well joined up multi modal public transport offer. Encourage active transport (including cycling).
- Frederick Street gateway should provide a welcoming, easy to understand first impression to the retail core.



Inspiration Images



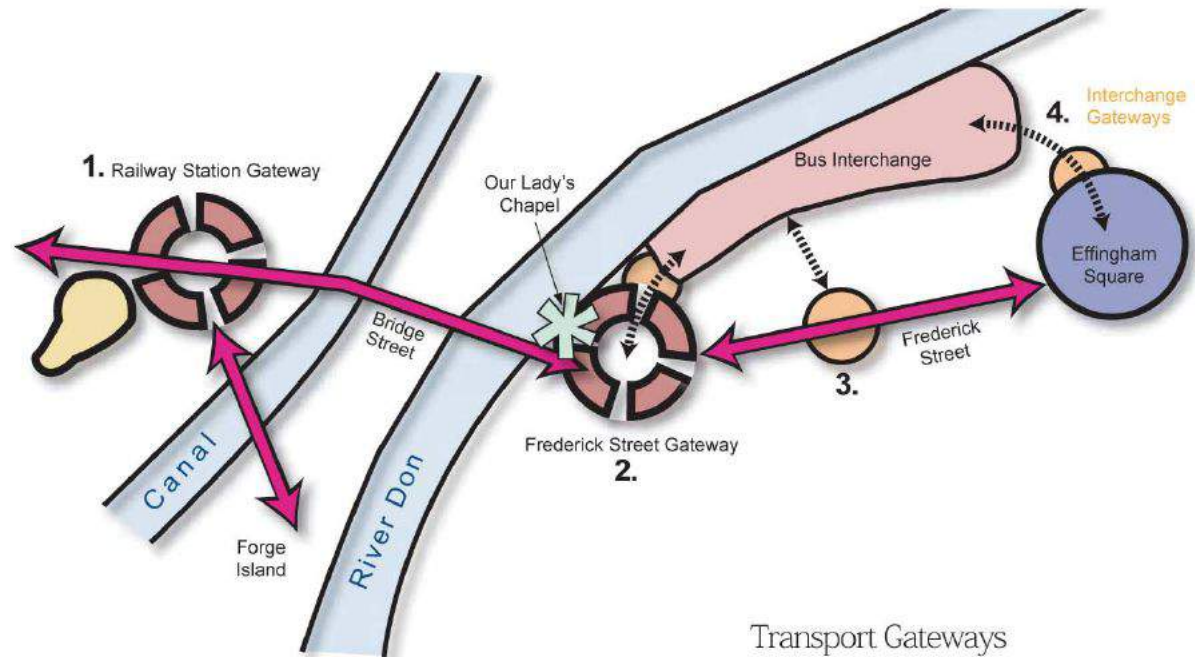
The following pages set out propositions and guidance for the Transport Area.



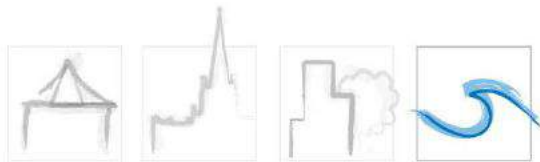
'Welcome to Rotherham' Transport Gateways

The diagram illustrates a number of pedestrian gateways from existing public transport stations. The baseline identified that they currently undersell Rotherham Town Centre and therefore these spaces should be improved to create:

- A welcome to Rotherham.
- Wayfinding signage.
- Clear links to public transport (multi modal offer).
- Opportunities for active transport (including cycle provision).
- Public art/feature to create a series of landmarks.



Transport Gateways



The following design principles are proposed for the transport gateway sites.

Gateway 1 (Railway Station):

- Improved environment in association with planned Train Station Upgrade. Focus on wayfinding and creating a clear pedestrian route along Bridge Street into the Retail Core and Bus Interchange. This route will be enhanced by the redevelopment of Forge Island and the improved access to the riverside.

Gateway 2 (Frederick Street/Our Lady's Chapel):

- Create an understated sensitive setting for Our Lady's Chapel, which is an important icon along the Rotherham Riverside Experience. The design should introduce a seating area/viewing deck adjacent to the chapel overlooking the river incorporating interpretive signage. At night the chapel should receive a complementary lighting approach to All Saints' Minster.
- Transform a vehicular dominant space into a positive pedestrian gateway leading into the Retail Core. Explore the opportunity for high profile public art to mark the junction. Remove highway clutter and pedestrian barriers, whilst safeguarding the functionality of the bus routes to and from the Station and along Corporation Street (see illustration left). Incorporate provision for cycle parking and signpost cycle routes (opportunity to create an artistic feature to highlight offer).

Gateway 3 (College Walk):

- Define the 'College Walk' mall junction with Frederick Street through the public realm treatment. A focal point feature could mark this important location. A canopy structure marking this busy pedestrian node would provide a landmark along Frederick Street and not conflict with pedestrian flow.

Gateway 4 (Bus/Interchange):

- The Bus Interchange entrance should interact with an improved Effingham Square and tie into the riverside walk. Incorporate provision for cycle parking and signpost cycle routes. Create quality, safe links and wayfinding between Town Centre core and new PCT facility.



Existing - Gateway 1.



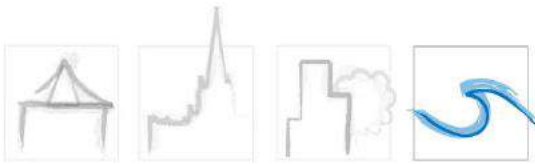
Existing - Gateway 2.



Existing - Gateway 2.



Proposed - Gateway 2.



Forge Island

Overview

- Existing Tesco supermarket and parking.
- River and Canal abut the island creating a distinct landform in the Town Centre.
- The weir is a major focal point but is hidden away from the Town Centre.
- Forge Island is proposed to be transformed into the 'Cultural Destination' in Rotherham (theatre, museum, library, café's and residential).

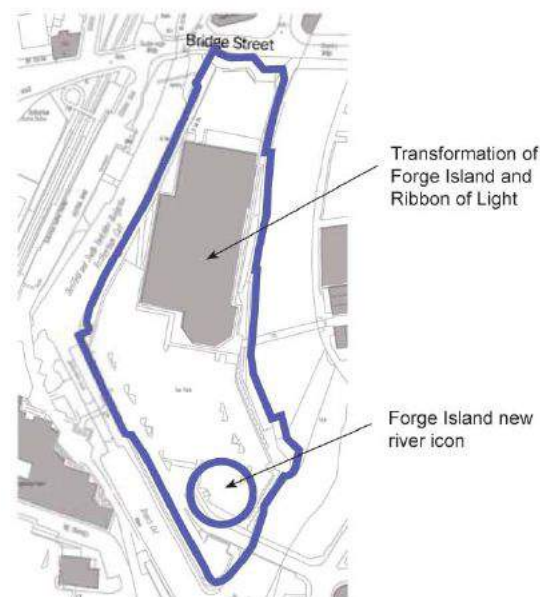
Existing Images



Forge Island

Vision & Inspiration

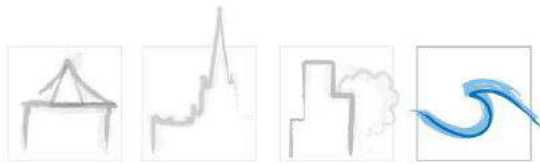
- A destination in its own right – a major new place to visit in the Town Centre incorporating a new Cultural Centre
- Tight, dense new townscape and urban form.
- Small scale, legible streets, links and yards – a people place.
- Traffic free aspiration (Service access only)
- A high quality public space should act as a focal point which interacts with the river and is visible from Westgate Demonstrator Project.
- A continuous waterfront promenade that should create a green edge.
- A contemporary place, focused on culture and the arts.
- Creatively tell the story of Forge Island and its rich heritage
- Forge Island should become a nationally recognised place and this should be highlighted with a showcase lighting installation that wraps around the entire waterfront edge.



Inspiration Images



The following pages set out propositions and guidance for Forge Island.



Forge Island 'Ribbon of Light' Display

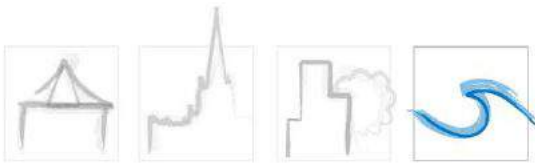
This ribbon of light display should be highly visual, active and become an attraction in its own right. In addition the Island can appear to be floating above the water! The architecture shown is notional and aims to evoke the contemporary nature of Forge Island's Vision.

Light powered by the River Don could make reference to the rich history of the water wheel that used to be situated there and provide a green sustainable energy source. Moving or changing colours of lights could be effected by the flow of the river to create various moods and ambience.

The new Arts Bridge which has been suggested to replace the existing bridge from Corporation Street to Forge Island should also incorporate lighting with opportunity for a seating or viewing platform, making an attractive contemporary feature that entices people to visit the Island and view the weir.



Proposed view of Forge Island 'Ribbon of Light' display.



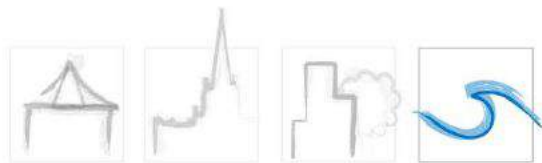
Forge Island 'New river icon'

A premium quality public space is proposed at the prominent southern tip of Forge Island and this should include a new river icon (refer to p56). Forge Island's exciting future as a cultural destination coupled with its history provides inspiration for this space. The river icon must deliver in terms of scale and impact and initial ideas include:

- A new water wheel to provide sustainable energy for public art and lighting
- Public art inspired by the Samuel Walker forge that created large numbers of cannons on the island



The southern corner of Forge Island is an unusual and recognisable feature in Rotherham, that can become the new 'Postcard View' of Rotherham - a landmark project.



Guest & Chrimes

Overview

- Former industrial area with full river frontage (opposite Westgate).
- It is proposed to construct new Council Office accommodation on part of the site, and the Council and the landowner are engaged in promoting the redevelopment of the remainder of the site.
- Challenging site in terms of flood alleviation issues.

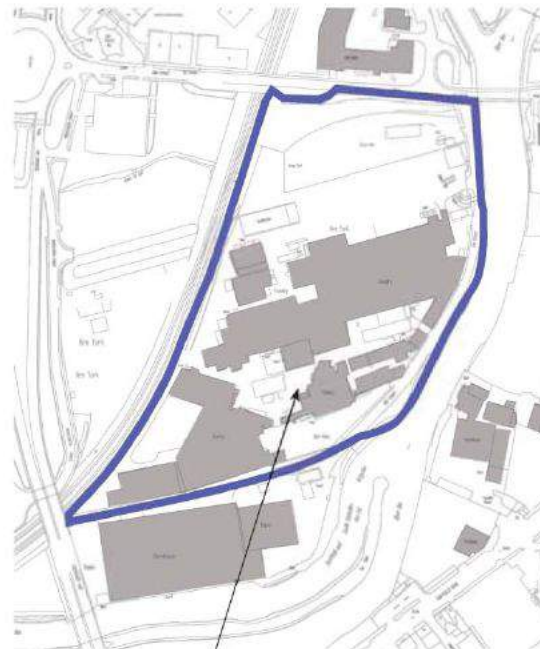
Existing Images



Guest and Chrimes site.

Vision & Inspiration

- Guest and Chrimes should be defined and incorporated as a new piece of Town Centre.
- A riverside promenade and series of public spaces that engage with the river.
- Riverside public realm proposals should engage with and echo the design guidance proposed for Westgate on the other side of the river.
- There is an opportunity to enhance and create a green character within the site and adjacent to the river.

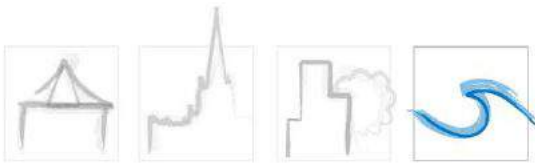


Guest and Chrimes redevelopment boundary

Inspiration Images



Informal riverside and new development.



Westgate Demonstrator & Westgate Riverside

Overview

- The Westgate Demonstrator Project which is currently underway proposes a leisure/commercial destination adjacent to the Weir featuring a flagship waterside public space 'the deck of cards'. The wider Westgate area is planned as a future phase of redevelopment, incorporating riverside access.

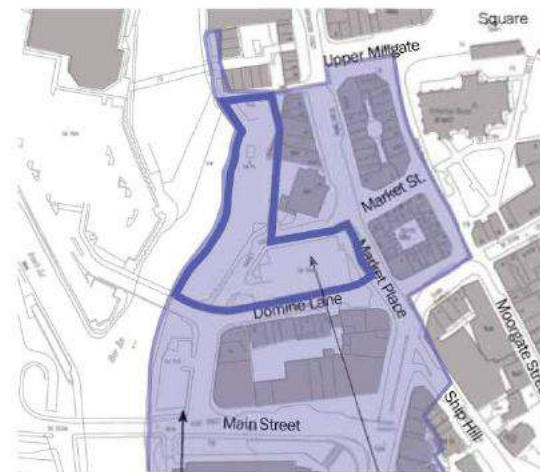
Existing Image



View of existing Westgate Demonstrator site.

Vision & Inspiration

- Westgate should be defined as a distinct accessible quarter.
- A Design Code is in place for the whole Westgate area that identifies four distinct character areas: Weir side, Westgate/ Main Street, Riverside and Hillside including mixed use, retail, commercial and residential.
- Proposals include an active riverside with bars and restaurants with linear riverside walk linking the 'deck of cards' public space and a public space on the southern end of Westgate.



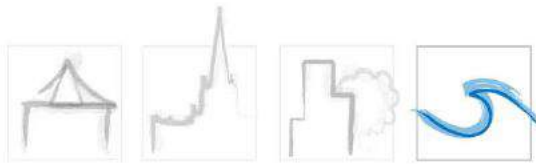
Design code area

Westgate Demonstrator project boundary



The 'deck of card's will provide a major public space adjacent to the weir with a pedestrian promenade planned for the remainder of Westgate's riverside.

(EDAW, 2007, Westgate Demonstrator Project).



Rotherham's Flagship Link between Guest & Chrimes & the Town Centre

Overview

- The existing link connecting the Guest and Chrimes site to the town centre is difficult to negotiate with narrow pavements and poor pedestrian/cycle legibility
- Within the context of redevelopment this link elevates in importance

Existing Image

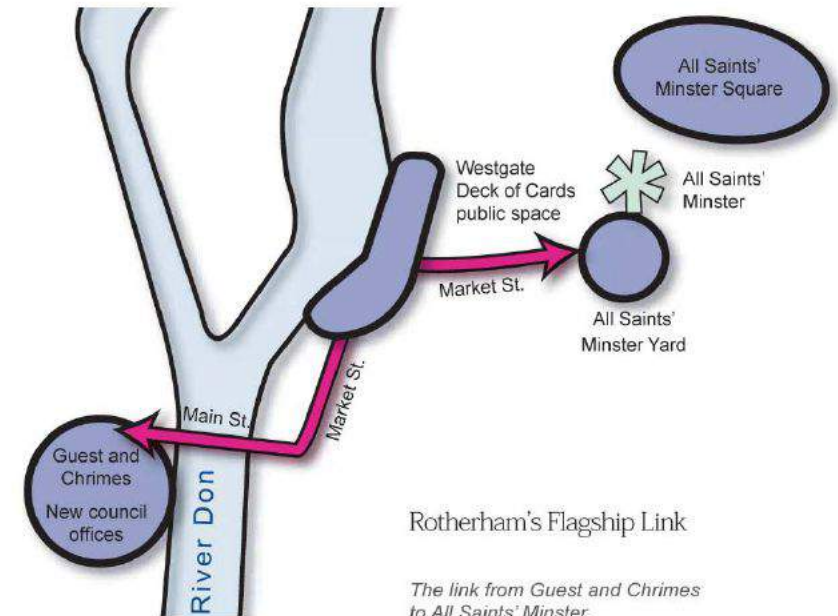
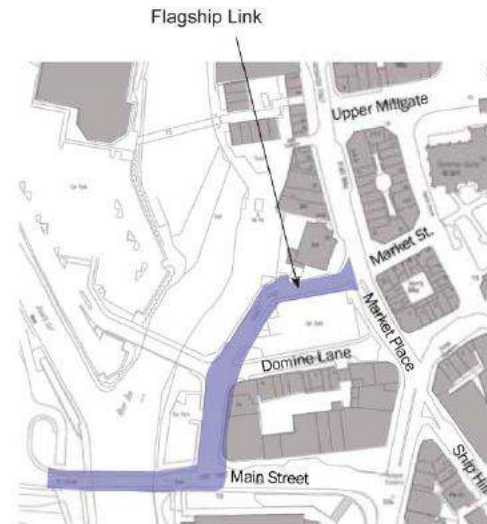


Vision & Inspiration

The strategy has identified the link from Guest & Chrimes development site into the heart of the Town Centre (All Saints' Minster) as its Flagship Pedestrian/Cycle Link. The link is intended to fulfil a 'civic' function and character linking the new Civic Centre planned for Guest & Chrimes to the Minster.

Treatment of the 'Flagship' pedestrian link should therefore:

- Provide a clear sense of direction and hierarchy of importance.
- Create a grand promenade.
- Use visual tools to guide movement.
- Provide an unique element to mark its importance, for example a bespoke lighting feature or/and the introduction of a distinctive red granite paving band (as a variation to the high/premium quality palette).



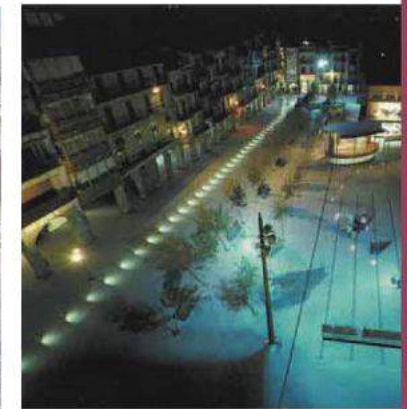
Rotherham's Flagship Link

The link from Guest and Chrimes to All Saints' Minster.

Inspiration Images



Sheffield has successfully highlighted its pedestrian link creating a strong pedestrian boulevard.



design guidelines

- Surface Treatment
- Street Furniture
- Signage and Information
- Lighting
- Street Trees and Planting
- Structures and Public Art
- Safety and Security
- Public Realm Specification Matrix

section 6

The Design Guidelines Section of the strategy develops each element of public realm in greater detail. It performs two purposes:

- Proposes good practice principles for public realm improvements (images from both Rotherham and elsewhere are utilised to illustrate general issues).
- Describes a Public Realm Specification Matrix, which establishes a working specification for future public realm schemes in Rotherham Town Centre.

surface treatment

The approach to surface treatments set out in the Vision proposes simplicity, elegance and subtlety avoiding excess changes of colour and materials and unnecessary detailing. A simple paving scheme creates harmony, a setting for street furniture, features, activity and surrounding architecture.

Paving Principles

- Provide a simple, harmonious floorscape with only subtle variations to provide 'sense of place'.
- Incorporate a strong 'kerb line' to provide a cohesive unifying and organising element.
- Take inspiration from the character of Rotherham's streets and spaces when specifying and detailing materials.
- Carefully consider transitions and interfaces between different streets and spaces.
- Incorporate accessibility requirements in line with national and local policies.
- Bespoke paving features can enliven the streetscape but should be incorporated where appropriate and with care to avoid trip hazards.
- Where possible avoid level changes especially in heavily used pedestrian areas.
- Improvements must comply with the documents that support the DDA (Disability Discrimination Act)

Specification & Laying Principles

Paving

Appropriate material selection:

- Materials should aim to set an environmentally sustainable approach for Rotherham:
 - Locally sourced materials are preferable both in terms of local distinctiveness and carbon footprint, however these are not always the lowest cost or indeed the most readily available option.
 - Green specification should be actively promoted (BREEAM & CEEQUAL ratings).
 - Reclaimed & Recycled materials should be considered where appropriate.
 - Opportunities should be explored to introduce permeable surface and sub-base systems where site conditions are appropriate for Sustainable Urban Drainage.
- High quality materials should be specified where possible. Natural stone products look, feel and age far better than concrete alternatives.
- Consider durability and functionality in specifying paving. Natural stone paving when laid and specified correctly is more durable than concrete. Areas of high pedestrian or vehicular impact should avoid light colours to minimise staining and resulting maintenance. The type of material selection is key where level changes, steps and slopes occur in terms of slip resistance and demarkation.
- Availability of materials is key to a successful, sustainable public realm. The majority of materials should be readily available to complete new works or repairs to existing paving. However, specialist materials should be encouraged in small areas to provide interest (such as Premium Quality Public Spaces) and arrangements should be in place for replacements to ensure longevity.



Specialist or bespoke materials should be encouraged in premium areas to provide variety and interest.

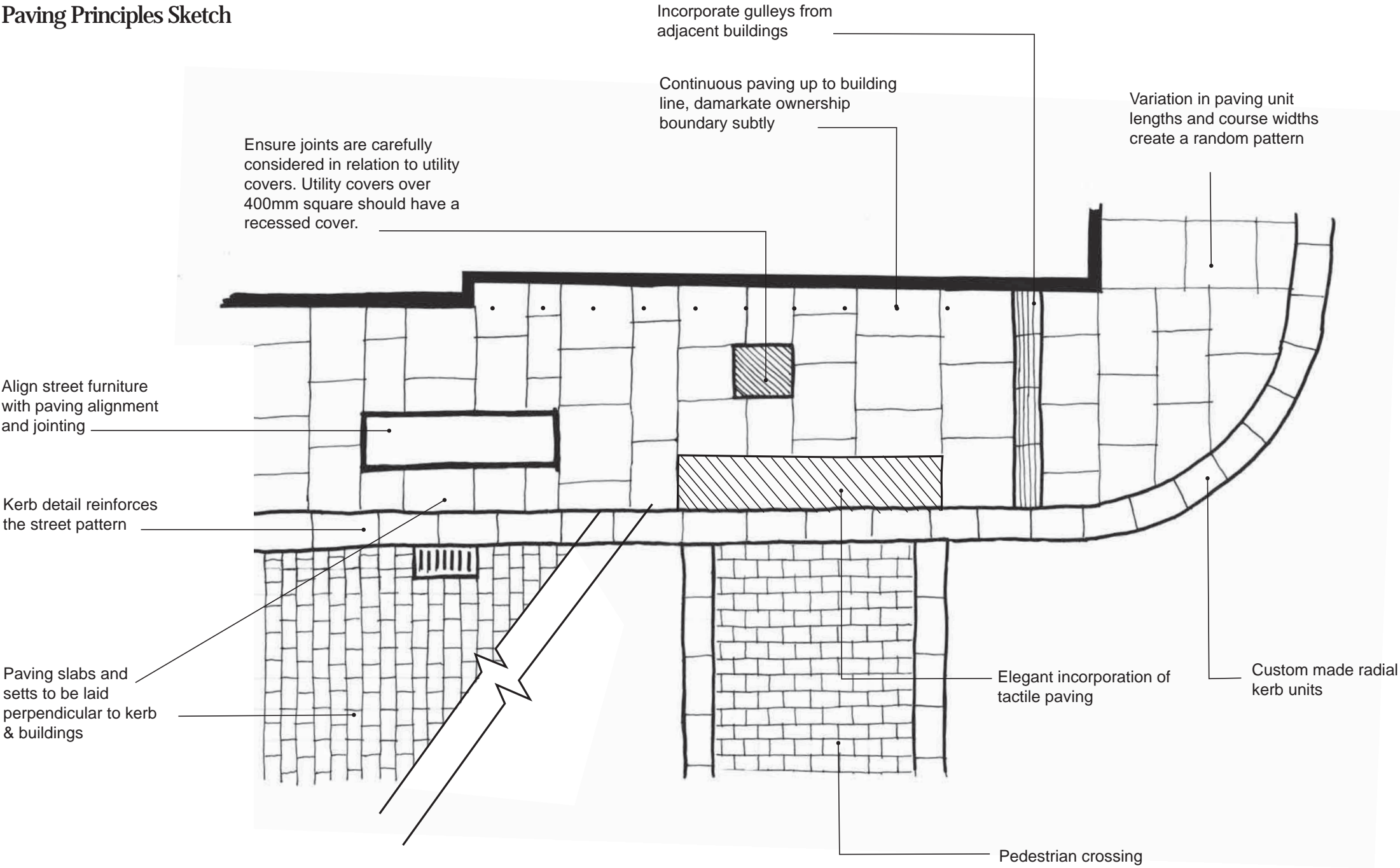


Darker sandstone paving slabs in heavier use pedestrian areas enable less cleaning and maintenance than lighter coloured slabs.



Varying width pattern, should be used for all concrete and natural stone paving to ensure small cuts of stone do not happen as seen in the example above.

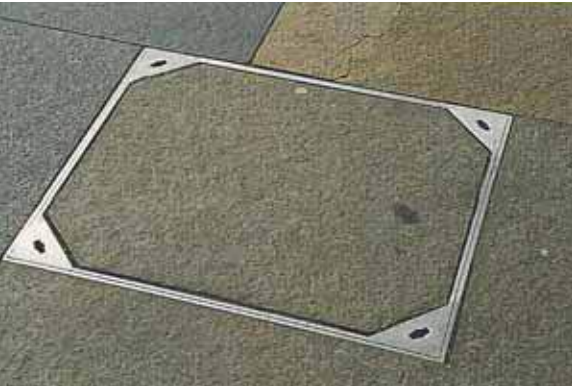
Paving Principles Sketch



The paving in the Town Hall Square as seen above is tidy and well thought out, from the placement of the drain to the transition of materials.



The channel above is appropriately integrated into the paving design.



The way paving is laid, orientated and its interface with other features is key to a high quality scheme- attention to detail is critical! Custom made units should be considered in difficult areas rather than trying to make standard units fit.

Paving:

Surface finish:

- Surface finish selection should consider location, function and character. A bush hammered/riven/cropped finish is appropriate to most applications of sandstone or granite, however sawn/fine picked finishes can be considered where a smoother finish is appropriate.
- Concrete products should be specified with a light textured aggregate finish with a chamfered or tumbled edge to avoid cracking.

Construction build up and jointing:

- Good jointing and foundation are key to a high quality, long lasting surface treatment. A rigid construction build up is recommended for most town centre applications, where the mortar bed and jointing compound binds the paving and setts units together. Ensure that pedestrian orientated spaces are designed to accommodate overrun of vehicles such as service/emergency use. Proposals should be guided by RMBC engineering standards.

Laying Principles:

- Clear demarkation of function/space:
 - The scale of paving units should be generally larger on pavement areas (such as slabs) and smaller on carriageways (such as setts).
 - Ensure clear distinction/demarkation in type, colour or scale of floorscape where transitions in use (pedestrian/vehicular) or levels occur.
- Minimise awkward paving cuts:
 - Paving layouts should be perpendicular to both the kerb and building line where possible to avoid poor alignment and awkward pieces of paving.
 - Paving should be carefully considered around utility covers, street furniture and building lines to minimise the number of cuts and ensure tidy detailing.
- Elegant paving patterns and layout:
 - Paving should generally be laid in varying lengths to provide and course widths to create random appearance.
 - Jointing should be 10-15mm for setts and 2-6mm for paving slabs. Joints should be carefully considered in terms of their aesthetics.
- Gullies and channels should be integrated into the paving design to create a unified appearance. Their positioning within the streetscape should where possible avoid pedestrian access routes and be flush to avoid a potential hazard.



The transition between concrete and natural Yorkstone paving needs to be carefully considered. The example above introduces a granite stone banding to delineate between the two types of paving, this is a tidy solution which ties in with the grey granite kerb.

Kerbs:

The kerb line is a key feature in the demarkation of floorscape and is important in maintaining the character, scale and proportions of a street. Kerb height can vary from a standard highway treatment to flush scenario to create a unifying and multi purpose element in the streetscape.

Kerb use principles:

- Unifying, recognisable element that runs through the town centre
- Delineate uses, boundaries and spaces
- Delineate between public and private
- Highlight changes in level
- Provide an organising element for street furniture
- Emphasise key pedestrian routes (for example the use of a strong band to emphasise flagship route from Guest and Chrimes to All Saints' Minster - refer to p105)
- A full height kerb should be used to create clear segregation between busy vehicular routes and pedestrian pavements. Shared use streets should aim to use a flush kerb or small kerb 'check' of 30-40mm to minimise segregation
- Granite is a preferred choice for kerbs due to robust high quality characteristics. The kerb should differ in tone and colour sufficiently from adjacent surfaces to clearly and safely demarcate function. For example the footway and carriageway in traditional streets and safe space and shared space in shared surface streets.

Carriageways:

- Bitmac is currently the predominant surface for vehicular carriageways in Rotherham Town Centre and the strategy continues to promote this for all heavy vehicular routes. However there are locations where it is appropriate to upgrade this with the aim of creating more pedestrian dominant environments and slowing traffic:
 - High quality and premium quality areas should receive upgraded bitmac with a granite aggregate as a minimum.
 - All streets should be considered for opportunities to use paving setts to define key pedestrian crossings, on-street parking bays, taxi ranks and bus lay-bys.
 - In high quality and premium quality areas, shared surface treatments should be encouraged in light traffic streets.
 - Cycle routes should be encouraged within the Town Centre highway network. Routes should be delineated in a simple way that provides a clear distinction while being sympathetic to adjacent surface treatments.



Good attention to paving and kerb detail, with contrasting colour kerb acting as a strong organising element throughout the scheme.



Flush granite kerb above shows clear demarkation of space and function with appropriate placing of bollards central within the kerb.



Old Vicarage Lane is a good example of where the use of recycled materials can be successful adding variety to the streetscape palette.



Avoid double yellow lines and garish road markings, and use more subtle delineation instead.

Creating Inclusive Places

All areas of the public realm should be welcoming and accessible and local authorities have a statutory duty to ensure this is provided where possible and improvements must comply with the documents that support the DDA (Disability Discrimination Act). The design of public realm improvements should actively consider the requirements of the elderly, parents with prams, those with mobility needs and visual impairment.

Consultation with local groups throughout the design process should be undertaken to address concerns and importantly balance varying needs and priorities, which can often conflict. Care must be taken to avoid the resulting scheme and facilities being compromised as this process should in fact add value and quality. Additionally it is recommended that guided tours of completed schemes could provide assurances to new users.

In addition to statutory requirements the following good practice principles should be considered in terms of creating an accessible, welcoming Town Centre:

Slope and Level Change:

- Where site conditions necessitate changes in level, or stepped access to an existing building is required, ramps not steeper than 1 in 15 should be proposed as an integral feature of the design solution.
- Exaggerated crossfalls across paving should be avoided, gradients between 1:30 - 1:40 is a good benchmark for good practice.

Traditional and Shared Surface Streets:

There is national consensus in our urban centres to reduce the dominance of cars and readdress the balance to put people first, creating more civilised streets. CABE (Commission for Architecture and the Built Environment), local authorities, design professionals/stakeholders and access groups are collectively considering how to address this. The debate is ongoing and projects will be monitored vigorously, to help develop a new design language that can meet varying needs and transform our town centres into safe, attractive and inspiring places for people.



Extract from Rotherham Access Audit Group Photographic Audit carried out to help inform the Strategy

Whether new schemes in Rotherham incorporate more traditional street configurations or shared surfaces, there are a number of design principles that should be considered to help create both attractive and user friendly places:

- Where pedestrian use abuts or converges with vehicular use a clear demarcation is required. The appropriate design solution can vary based on the intensity of traffic and the emphasis of the space, however this can be achieved through use of strong kerb lines that contrasts in colour and tone with adjacent surfaces and considered use of varying kerb heights from flush to full kerb.
- Flush 'At grade crossings' should be provided in all possible instances. In addition, 'Carriageway' surfaces should contrast in tone and size of paving unit from adjacent pavements.
- Tactile paving is required to denote changes in levels or pedestrian crossings. The layout and materials for areas of tactile paving must fulfil the statutory requirements as a minimum but should also be elegantly designed to fit the design and character of the street in terms of colour, paving cuts and materials.
- The use of contrasting colours and tones in tactile paving is of particular importance to visually impaired users, however poor design can compound the 'patchwork' public realm currently in Rotherham. In High Quality and Premium Quality areas the use of custom made tactile paving in natural stone materials should be promoted.



Access ramps should be an integral part of a scheme and should be made into a feature where possible.



Stud tactile pavers or brass studs are required at pedestrian crossings and corduroy at changes in level. These should be designed sensitively and sensibly. Studs should be raised enough to serve their purpose but not overtly proud. Where possible tactile units should be made of matching materials to the surroundings.



The image (left) shows good use of paving in a pedestrian crossing, however the tactile paving is insensitive and a more subtle attractive use of natural stone tactiles or crossed brass studs as seen below would have been more appropriate.



street furniture

Street furniture is a key feature within the public realm both in terms of its functional role and its ability to create distinctive identity to a place. The vision for Rotherham is to develop a style of furniture which can provide harmony throughout the Town Centre, whilst allowing the flexibility for variation and innovation.

Street Furniture Principles

Quality & Harmony

A step change in quality for Rotherham Town Centre's street furniture is proposed through the standard quality palette (Refer to p58). The range is simple, elegant and inspired by recent improvements to the Town Centre, with a matt black finish and predominantly metal construction. This approach is intended to raise the quality benchmark providing a palette which will blend rather than contrast with the Town Centre's existing furniture. Improvements within high and premium quality areas should compliment this framework by introducing site specific variation to the standard quality palette and bespoke 'signature' elements where the character and profile of the location demands such a response.

Within a defined space the street furniture should be seen as a 'family' of complementary elements. Variation throughout the Town Centre is inevitable and desirable, the key to a harmonious and unifying style of street furniture is the interface between varying styles or families of furniture. Where two families converge, it is essential that they are purposefully located to create a clear distinction between the two. For example a main street and its furniture may take precedent over a secondary street or an adjacent public space. In this scenario the main streets furniture should run continuously with the other families of furniture setback. Alternatively a public space and its family of furniture may break an adjacent streets furniture continuity for impact defining the public space as a major destination.



A lack of co-ordination between lighting elements and paving. Planters are dated and tired inhibiting movement through a space.



Street furniture can be used to create a distinctive public realm and give a strong identity to a place.



Bespoke elements and features should be an essential ingredient in premium quality spaces to create inspirational, attractive streets and spaces.

Variation & Innovation:

A harmonious approach should not limit design creativity, in fact the strategy has failed if it can only achieve 'uniformity'.

The high quality palette recommends the same furniture as the standard quality palette as a baseline. However, variation should be encouraged where appropriate to create interest and identity. For example the palette may take on a different character to define a public space or a distinct area such as the Heritage Core. These variations should be broadly coherent with the Standard quality palette in terms of colour and materials.

The premium quality palette proposes bespoke elements throughout to allow creative license for these locations to become unique, unmistakable places. It may be equally appropriate for street furniture to contrast with or instead complement adjacent improvements.

Layout

Reduce Clutter:

- Street furniture should be kept to a minimum and careful consideration given to quantity, location and need.
- A paving feature, such as a kerb line can help organise furniture.
- Where possible elements including lighting, signs, shelter and CCTV cameras should be integrated.
- Less is more in terms of furniture associated with vehicular routes. Latest guidance including the Manual For Streets (Department for Transport, 2007) highlights the benefits of reducing unnecessary signage and guardrails and barriers.

Define space and desire lines:

- Street furniture should be used to help create and define space.
- Good design and careful siting of furniture can create an attractive, functional and safe environment.
- Pedestrian desire lines and routes can be emphasised and guided by street furniture. Well organised furniture creates legible environments for all users including the visually impaired.

Design out awkward spaces and conflicts:

- Street furniture should be carefully arranged with street cleansing and maintenance in mind:
 - avoiding awkward corners and overhangs.
 - avoid locating elements against walls.
 - allowing 1-1.5m between elements to allow access to street cleansing vehicles.
- Street furniture should be carefully arranged with disabled access in mind:
 - allow sufficient space and avoid obstructions adjacent to seating areas to allow for wheelchairs and access.
 - avoiding obstructions to key desire lines by carefully locating and demarkation of temporary features such as market stalls and street cafes.
 - defining pedestrian crossing and transitions.



Important views of landmark features such as Our Lady's Chapel seen above is obscured by street furniture clutter.



The de-cluttering of street space as shown in the proposed photomontage for Frederick Street above, creates a simple, well defined space that is legible and attractive.

Street Furniture Elements & Application

Specification Principles:

Street furniture is an essential component of successful public realm. The specification of these elements needs to consider the appearance and style, functionality and durability:

- The appearance of an element of street furniture should consider how it ties in with the town centre wide style.
- The functionality of street furniture should be user friendly catering for a range of users.
- Street furniture should be durable and constructed of robust materials that will withstand vandalism and damage, fading or staining of surrounding surfaces. Materials should aim to set an environmentally sustainable approach for Rotherham.
- Be considered in terms of maintenance and repair:
 - Avoid excessive overhangs or awkward corners that attract litter/grime and are difficult to clean.
 - Custom elements should be designed in such a way that components can be replaced if damaged to avoid replacing the whole piece.
- Opportunities for responsible timber purchasing should be explored.
- Furniture selection should consider access issues in consultation with local access groups.

Seating:

A range of seating opportunities should be provided to cater for all needs and occasions, especially to provide regular stopping points in high use areas of the Town Centre. Locations should consider provision of shade and shelter for all year round use.

Formal Seating:

Benches are versatile in terms of their use and style, however a variety of options should be included to meet a variety of peoples needs.

The majority of benches should incorporate a backrest and armrest to cater for accessibility and comfort. Backless benches are only appropriate where the space requires a double sided approach.

Informal Seating:

Town Centres are enlivened by activity and people watching. While formal seating plays an important role, informal opportunities bring a new dimension to the public realm and how it is experienced:

- Sculptural seating elements (for example serpentine granite seats), offer attractive 'short stay' seating which may be appropriate for busy shopping streets.
- Spontaneous seating opportunities (for example steps or terraces) promote interaction and personalisation of the environment and can be planned for in the public realm design.



Existing street furniture in Frederick Street and Effingham Square is tired and dated.



Benches as seen in All Saints Square are the design basis for future proposed seating (i.e. timber slatting and matt black finish).



Seating should be in keeping with recent improvements in Rotherham (but moving away from 'a heritage style' with simple contemporary shapes).



Armrests should be provided as an option.



Informal seating opportunities enliven the streetscape.

Litter Bins

Litter Bins should have sufficient aperture to take a 12" pizza box and should include cigarette stubbing facilities and chewing gum dispenser integral to the bins. They should be no less than 100L in volume, have a self locking door and be fixed securely to the ground. The locations should be carefully considered to ensure good distribution, as a starting point litter bins should always be located by bus shelters, seating areas and focal spaces.

Bollards

Bollards should be simple and elegant in form and only used where absolutely necessary (i.e. where the prevention/protection from traffic is required). Alternative options should be considered such as cycle racks, benches or sculptural features. Banding should be incorporated to assist visually impaired users.

Pedestrian Guardrails

Pedestrian Guardrails should be avoided unless absolutely necessary within the Town Centre. They often dominate the streetscape and can intrude on pedestrian/cycle usage. More passive segregation methods should be explored first, such as paving design, planting and bespoke features. If formal barriers are indeed required, use should be to a minimum, carefully located and a decorative 'Town Centre Guardrail' can be used to create to upgraded character.

Cycle Racks

Cycle Racks should be simple, functional and located to avoid obstruction. They should have a 'duracast' finish to resist damage to cycles and cycle racks. They should be located in prominent, well overlooked locations to encourage use and passive policing

Fingerpost Signage

Fingerpost Signage should be simple and elegant in form (refer to Signage and Information for details).

CCTV Cameras

CCTV cameras should be building mounted, or combined with other items of street furniture, such as lighting, where possible to reduce the number of columns required. CCTV columns should be sited to avoid conflict with trees, to avoid pressure to remove trees to improve effectiveness off CCTV.

Tree Guards

Tree Guards should be provided where necessary, but there is no presumption that all trees be guarded.

Tree Grilles

Tree Grilles should include removable rings/sections that can accommodate tree growth.

Other Street Furniture

Other Street Furniture such as parking meters, cycle lockers, recycle bins, bin stores, rising bollards, signal control boxes and CCTV mounting poles should also be compliant with the proposed street furniture palette by using the same colour and style if possible. Bus shelters, telephone boxes and portable toilets are outlined in greater detail in 'Structures', later in this section but should also compliment the street furniture style.



The street furniture 'style' should be simple, elegant, a sensitive blend of modern and traditional, constructed of metal with a matt black finish.

signage & information

Legibility and movement through Rotherham Town Centre can be improved by a variety of methods, both 'implied' (such as lighting, paving, visual links) and 'formal' (such as signage). A comprehensive signage approach is proposed, (intended to support 'implied' methods) that presents information in a clear and efficient manner to all. This can be achieved through rationalisation of highway signage, clearer pedestrian signage and information points and a clear strategy and enforcement of street advertisements.

Traffic Signage Principles

Excess traffic signage is confusing to drivers and can create obstacles for pedestrians. The design, layout and application of highway signage must comply with statutory requirements, however within this framework there is scope to think creatively in terms of achieving the desired function and avoiding clutter and negative impact on the pedestrian environment. In fact, current thinking on reduction of signage clutter and lessons from high profile case studies such as Kensington High Street is feeding into national guidance including the Department for Transport's Manual for Streets.

The following principles are suggested in order to rationalise highway signage:

- Remove redundant signage.
- Rationalise signs onto one unit to avoid multiple poles. Poles should be dark in colour to recede into the street scene.
- Explore opportunities to tie in with the Gateways Initiative signage to create a town wide design language. Town Centre Street name signs incorporating the Rotherham coat of arms could follow the precedent set by local centres through the borough.
- Integrate signage where possible into the existing urban fabric by locating on elements such as walls or railings.
- The removal of back plates to traffic lights can help to reduce their size.
- Large strategic signs should be replaced by small 'town centre scale' signage.
- Double yellow lines are particularly intrusive and should be avoided. Where possible small wall or pole mounted signs should be used for enforcement zones.

Street name signage

Street name signs could be a variation of the 'gateway signs' seen throughout Rotherham, but enhanced to distinguish them from out of town centre variants. Street name signs within the Town Centre could have a coloured coat of arms.



Creative design of traffic lights and combining elements to reduce clutter should be considered.



Signage plays an important function in aiding pedestrian legibility. The use of the Rotherham logo on all signage does provide cohesiveness and acts as a continuous element, however the gold lettering and heritage style of the signs, looks fussy and dated.

Pedestrian Signage Principles

Pedestrian signage is important to help visitors and residents orientate themselves within the Town Centre. Pedestrian signage predominately includes fingerposts and information boards, however alternatives such as inserts in paving and lighting projections can be explored to add richness. To be effective, signage should incorporate pictograms and convey the minimum level of information required to maximise simplicity, clarity and memory.

The location of pedestrian signage should be carefully considered both in order to minimise street clutter and maximise the user friendliness of the message that is being conveyed.

The following hierarchy is proposed:

- Information Boards and Maps should be located at key pedestrian arrival points to the Town Centre including car parks, public transport gateways and pedestrian gateways
- Simple fingerposts should be used to highlight directions to a small number of key destinations to avoid information overload. These should be located at pedestrian convergence points and public spaces
- Site specific information such as marking places (e.g. The Crofts area) and points of interest (e.g. the River Don, Our Lady's Chapel) should be designed in keeping with surrounding. Options might include, bespoke signage, public art incorporating information or paving features. As an example, the Green Connections Concept (Vision Section p69) suggests public art inspired signage.
- Opportunity to create a 'Town Centre Trail' using interpretive educational signage and public art.

The following principles are suggested for pedestrian signage:

- Signage should be clear and legible for all including the visually impaired.
- Lettering should provide a clear contrast with the background.
- The typeface should be a simple and modern font.
- Symbols should be used to complement or even replace the need for text (catering for all nationalities and people with literacy/reading difficulties).
- The use of Braille or raised text for partially sighted pedestrians should be considered on information boards/maps along with voice activated information at pedestrian crossings.
- Information boards should be large enough to provide a good level of information but should be in scale with its surroundings.
- Fingerposts should be simple and elegant and keeping with the street furniture palette (refer to p131).



Informative, simple, well designed information boards should be used with a matt black finish to complement street furniture palette.



Simple elegant finger posts with a matt black finish should be introduced to match the street furniture palette.

Shop Frontages

While it is not the purpose of this document to propose specific guidance for shop frontages, it must be recognised that these have a significant impact on public realm. Of particular importance is the contribution that a building frontage makes to the public realm, the permeability of a frontage and its ability to encourage activity and safety (such as along alley pedestrian links) and consideration of the visual impact of ‘back of house’ activity, service areas and features including air conditioning units or vents.

The baseline analysis and consultation identified that high quality architecture is often let down by ground floor treatment, therefore the Public Realm Strategy echoes the Principles of Good Shopfront Design set out in the RMBC Rotherham Shopfront Design Guide (July, 2006).



Active frontages that are interesting architecturally and provide a link between internal and external spaces play a key role in enriching the townscape.



Security shutters are inactive banal frontages and have a negative impact on the public realm.



lighting

In essence lighting plays a functional role within a town centre, fulfilling safety requirements during the hours of darkness. In addition it can have a dramatic influence on the character and feel of streets and spaces and their resulting success as a place. While lighting is at its most influential at night, the infrastructure such as light columns are an important component within the public realm by day.

There is a need to consider lighting throughout Rotherham Town Centre to create a unified and sustainable approach and two main types of lighting are explored here. 'Everyday' lighting (both pedestrian and vehicular scale) and 'Impact' creative lighting (ranging from small scale accents through to major light installations). In addition, the vision proposes the Rotherham Lighting Experience as a high profile public art attraction expanding the Town Centre's use after 5pm and creating a showcase for Rotherham. This focuses on high profile 'lighting destinations and routes' and temporary installations.

Lighting within the Town Centre should:

- Ensure that statutory requirements are met.
- Enliven the Town Centre environment creating a reason to visit.
- Respond to Rotherham's built form and heritage evoking sense of place and celebrating icons.
- Ensure that streets and spaces are sufficiently well lit to create active frontages and promote personal safety in accordance with Secured by Design principles and Regulations BS5489-1 and BS EN 13201 - 2003, Lighting of roads and public amenity areas.
- Ensure that the overall safety of a space is integral to the lighting approach in addition to the impact it will have on CCTV footage.
- Encourage creative thinking to enhance a vibrant, exciting public realm experience.
- Respond to the scale and function of various streets and spaces.
- Demonstrate sustainability through use of energy efficient, well directed and located fixtures which maximise impact and minimise light spill. Use renewable lighting options where possible such as solar powered units. White light should be used to improve the aesthetic character of the public realm whilst improving CCTV visibility.
- Create longevity through specifying robust and highly vandal proof fixtures and fittings.
- Emphasise preferred pedestrian routes to empower people, avoiding confusion.
- Provide high quality seasonal displays (such as Christmas Lighting). Opportunity to improve current infrastructure such as catenary wires.

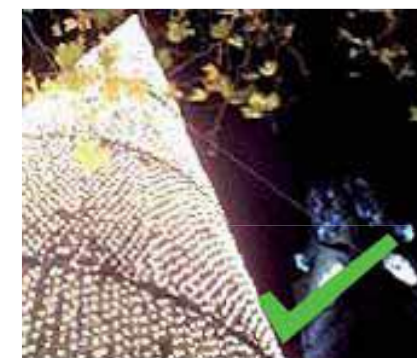


The heritage style lanterns at the entrance to the Minster are more attractive and appropriately placed than the lighting fixture on the right.



Ground lighting illuminates routes and can aid legibility in an attractive creative way.

Successful example of lighting incorporated into seating and steps to make attractive features.



Manchester Christmas Lights - designed by specialist lighting contractor. Traditional lights were replaced by low energy LED bulbs to reduce the electricity bill.

Statutory Lighting (Everyday Lighting)

- Lighting must take into account the statutory requirements required to illuminate streets. Two methods can be used to achieve basic lighting levels:
 1. Vehicular/pedestrian scale lighting columns which can light a space evenly without need for additional methods. Creative lighting can be added for effect, however are not required to meet basic lighting requirements.
 2. A combination of methods that cumulatively light a space (e.g. building mounted flood lighting, low level pedestrian lighting bollards, in ground uplights). This approach is often characterised by less dependence on light columns.

'White light' should be introduced as a baseline for all Town Centre lighting, following successful schemes in residential areas of Rotherham. White light has numerous benefits when compared to the typical high pressure sodium 'orange light' including safety and security, aesthetic appeal and running cost.

Add ons to light column's such as hanging baskets, banners, signage and CCTV should be actively encouraged to enhance the street scene and reduce street clutter and unnecessary poles, however the design of these should be carefully considered to maintain the integrity and elegance of light columns.

Where possible it is desirable to create bold avenues of light fixtures but a balance needs to be struck between uniformity, lighting levels and the location of columns. Ideally lighting columns should be evenly spaced along one side of a road to provide a sense of order and uniformity. The design of lighting columns should be simple and elegant avoiding fussy detailing.

All lighting provided should have a colour rendering index of 60 or above. Light columns should incorporate necessary equipment to support creative lighting where possible, such as sockets/timers for Christmas lights. The life and maintainability of the units together with energy consumption are also of prime importance.

As part of the design process sustainable lighting options shall be considered in the first instance on all Town Centre schemes.

Safety and security has been considered and consultation has occurred with the police liaison officer, where it was suggested that lighting schemes be boosted at certain times by remote switching methods.



Creative Lighting (Impact Lighting)

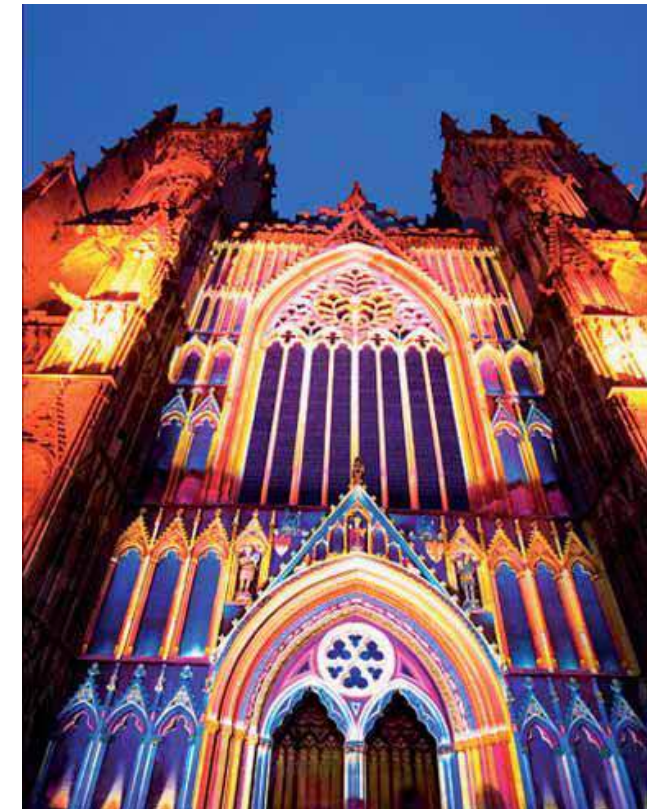
The introduction of creative lighting can have a major impact on Rotherham Town Centre. It makes a statement of Rotherham's renaissance, creates memorable places and encourages people to return to the Town Centre after 5pm. The 'Rotherham Lighting Experience' (refer to p73) will be underpinned by the all year round lighting features (such as landmark buildings and public art) and provide a platform for temporary installations and lighting festivals. Public realm improvements should accommodate flexible and adequate electrical/lighting infrastructure to create opportunities for 'Impact ' lighting. As part of the design process sustainable lighting options (such as self powered or low energy) shall be considered in the first instance on all Town Centre schemes.

A variety of possibilities are available to create impact through:

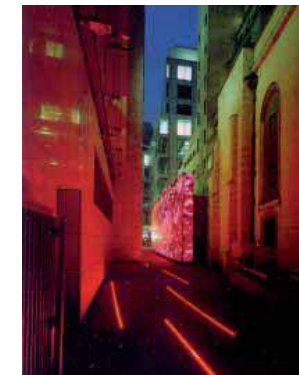
- Uplighting or illuminating trees, seating and features.
- In ground uplights to mark paving features and pedestrian routes.
- Illumination of buildings.
- Bold public art installations such as projections and interactive features.
- Use of colour where appropriate to transform the night scene.



Creative lighting should be used extensively in premium quality areas to invigorate and create dramatic exciting spaces.



Successful creative lighting of York Minster.



street trees & planting

Trees and planting in urban areas have a calming and softening effect on the built environment; they enliven space, provide colour, structure, seasonal interest, shade and reduce wind speeds. In addition the environmental benefits are significant, providing an ecological resource, absorbing pollutants and evaporative cooling. Above all, when implemented well, trees and planting help make great places for people to enjoy and spend time.

Tree guidelines

Trees should be considered and planned for at the outset of future schemes and plot development. They should be actively encouraged in Rotherham Town Centre but the selection and location of them should be carefully considered for maximum benefit. The existing trees in the Town Centre should be retained where possible to ensure the existing level of greenery is maintained.

Tree planting should be used to:

- Define key routes and desire lines (as part of the green connections strategy - refer to p69).
- Create community focal points.
- Create living landmarks and a valuable ecological resource.
- Create high quality landscape settings with a sense of place to attract investment and tourism.
- Create a place of well-being.
- Reinforce the scale and proportions of a street or space.
- Screen or filter unattractive views.
- Buffer pedestrians from busy vehicular routes.
- Enhance the character and distinctiveness of a street or space.
- Provide shelter from wind or rain, or reduce glare and reflections.
- Improve micro climate through air quality and reduction of noise levels.



The light airy canopies of the trees on Upper Millgate Street offer leafy shade and are an attractive feature of the public realm. These trees are appropriate in form and scale, and define an important route towards the Minster.



It is important that trees do not obscure views of important facades or focal points.

Tree planting should avoid:

- Obscuring attractive facades and features (can instead frame and compliment).
- Interfering with pedestrian desire lines.
- Having an adverse impact on safety and security by avoiding the creation of hidden places, obscured sight lines, lighting or CCTV cameras.

Tree specification:

Form:

- Species should demonstrate a tidy overall form with the branch network providing a strong architectural form when visible in winter. Native species should be considered ahead of other options.
- Species selection should be adaptable where places with their own particular identity may need to be developed.

Size and Scale:

- Where possible trees planted in town centre environments should be semi-mature specimens to provide an instant impact and be more resistant to damage or vandalism.
- Trees should have a clear stem of at least 1.8m. This minimises risk of injury or vandalism.
- The ultimate height and spread of species at maturity should be considered to ensure trees remain appropriate to their surroundings. However, this may not necessarily apply where the tree dimensions may need to be controlled either by short or variable term rotation replacement planting schemes or pruning regimes designed to constrain natural tree growth.

Canopy density and leaf size:

- Open and airy canopies are preferred in town centre environments to allow light penetration.
- Smaller leaved trees are preferable in town centres. In autumn, large leaves lying on the ground represent a slip hazard in the rain. Smaller leaves disperse more easily however proper maintenance should be provided for leaf collection to minimise risk.

Planting principles:

- Tree pits should be designed for urban environments and consider use of 'urban tree soils', insitu hoses/irrigation systems and tree guards to minimise vandalism until establishment.
- Public utilities are the main consideration/constraint in locating trees in urban environments and solutions such as relocation can prove unfeasible in financial terms. The introduction of appropriate root barriers to prevent damage to adjacent utilities can help in certain instances. Grouping utilities in a common trench can be considered in new developments to plan for street tree incorporation.
- Street trees should predominantly be planted at ground level in suitable surrounds (such as a tree grate) to minimise pedestrian obstruction.
- In certain instances, raised tree planters with appropriate tree species can be used where they fulfil additional functions such as informal seating and screening. They should also be designed as an integral part of the public realm, and their location should avoid obstructing pedestrian desire lines and site lines.
- Moveable tree planters can be considered when there is a definite need for regular moving of the tree, such as a public space that incorporates events or markets, or where it answers a design solution for a space (as illustrated in proposals for Effingham Square, refer to p85).

Maintenance and Management:

- The implications of developing and sustaining a population of public realm trees should be identified and managed. This will include considering systems for variable term replacement planting schemes, more intensive pruning regimes, safeguards to protect trees against damage and leaf collection.

Planting Guidelines

To complement the green structure created by street trees a number of other forms of greenery can invigorate Rotherham Town Centre's environment. Traditional bedding displays play an important role and should be encouraged as they provide year round colour and variety. This approach should be focused on high profile locations creating greater impact and quality. In addition, in recent years a more sustainable emphasis has emerged through planting swathes of perennials in urban areas, creating drama with minimal maintenance.

The Green Connections concept (refer to p69) identifies the opportunity to bring innovation and variety into planting schemes creating a living green infrastructure. The following suggestions may be appropriate for the 'Green Connections' identified:

Bold planters and Hanging Baskets:

- Planters (moveable or static) should be in limited locations only, and be considered when designed as a integral part of the public realm. Their location should avoid obstructing pedestrian desire lines and site lines. 'Off the shelf' solutions should be avoided as they often appear out of scale and context in an urban situation. Where appropriate, planters should fulfil additional functions such as informal seating and screening (such as Effingham Square proposals where raised planters buffer the seating area from the busy road, refer to p85).
- Large areas of shrub planting, should be avoided in an urban context due to potential lack of variety and interest. Maintenance requirements can be ominous and become safety concerns in terms of obstructing views and creating 'hidden spaces' that create pedestrian barriers.
- Hanging baskets are a much appreciated asset to town centres and when approached creatively can create and provide a bold statement.

Intensive, Extensive and Semi Extensive Green Roofs:

- Rooftops are generally an untapped and plentiful resource in urban centres. Green roofs can bring economic, social and environmental benefit and Rotherham can promote its green agenda through them. Moorgate Crofts Business Centre green roof is a recent success story.
- A variety of options are available on both existing and new buildings ranging from providing accessible green public spaces (Intensive) to 'plant focused' roofs (Extensive). For further information refer to www.thegreenroofcentre.co.uk.

Green Walls and Screens:

- The introduction of green walls and screens utilises the many blank or unattractive canvases that urban environments inevitably possess. They provide an innovative way to turn these negatives into a positive contributor to the townscape and can bring a fun element to the public realm.

Living Sculptures:

- A whimsical and organic addition to the public art arsenal. Examples include sculptural green domes or dramatic gateway planting that change with the seasons.



Moorgate Crofts Business Centre



Examples of green walls and screens which are an effective way of creating green links whilst screening unattractive buildings or facades.



Bold planting creates drama.

Structures & public art

Structures

Climate has a major impact on the way we experience urban environments and must be considered holistically as part of public realm improvements both in order to provide functional and attractive shelter. In order to attract people away from climate controlled shopping centres and back to their Town Centre, the experience must be as comfortable as possible, whether waiting for a bus, using cash points, relaxing in outdoor cafes or just window shopping.

Structures can contribute enormously to street life and activity throughout the year including market stalls, awnings, canopies, cafe umbrellas and bus shelters. Although variety in these elements enlivens the street scene, consideration should still be given to their cohesiveness with the proposed street furniture style and colour.

In addition to the everyday benefits of structures, they can also become landmark features and the Vision and Framework Sections consider opportunities including the creation of covered shopping streets.

Poor design of structures can be detrimental to their surroundings and its desired purpose. The big screen in All Saint's Square provides a popular function but has been highlighted by a wide range of the community as a negative feature due to its blank poor quality back being a major focal point along a number of shopping streets. In response, a project in coordination with the Town Centre Manager is underway to consider a new location for a screen.

Market stalls

Street activity can create vibrant, pleasant places to spend time. Temporary markets such as Sunday markets, European markets, farmers or craft fairs can enliven a space and entice people into the town.

It is important to carefully consider the layout of market stalls. Key considerations include:

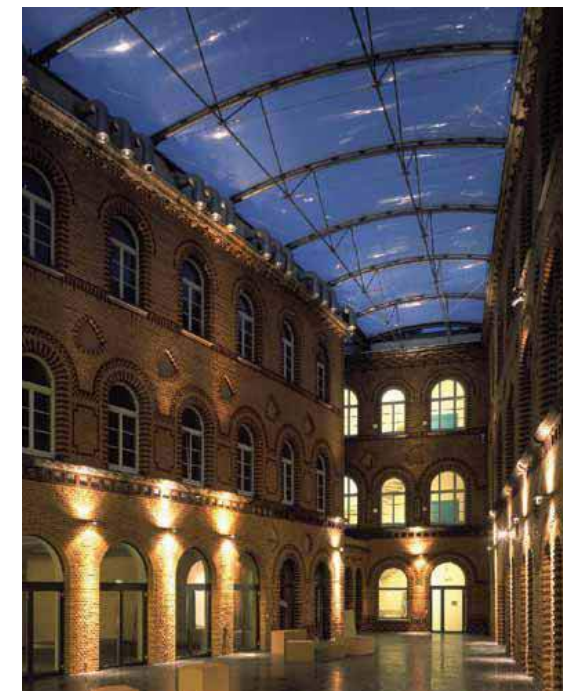
- Avoid conflict with pedestrian flow
- Keep viewpoints clear to maintain visual coherence
- Potential negative impacts on adjacent retailers (such as access)
- Temporary market stalls can appear ad hoc and untidy. A holistic design of canopies, signage and stalls can create an attractive feature and widen appeal.



Temporary structures such as the market stalls add colour and richness to the public realm.



Structures can be iconic and sculptural elements within the public realm



Structures can completely change the character and environment of a space through partial or complete enclosure.



The bus shelters above suitably match the street furniture palette within that location. It is important that proposed structures for Rotherham match the existing public realm palette through with a matt black finish etc..

Public Art Installations

Art is a celebration of culture and community and should be an integral part of Rotherham's public realm. It has many benefits from being a catalyst for or product of urban regeneration and inward investment. It encourages sense of pride and an expression of a place and its heritage. Indeed it can become a focal point evoking discussion, imagination and even controversy. RMBC are currently advancing this agenda through an emerging Public Art Framework, with a number of exciting projects under development.

Public art ideas weave through the Vision and Framework Sections of the Public Realm Strategy including Forge Island Ribbon of Light, The River Don River wall and The Green Connections. The Rotherham Lighting Experience (refer to p73) has emerged as the iconic public art project for Rotherham Town Centre.

The following principles are recommended to gain maximum impact from Public Art:

- Projects should be advanced in partnership between artists/ professionals and the community.
- A variety of permanent work including sculpture, water and lighting should be commissioned.
- A programme of temporary installations focused on Lighting and Green Connections. Consider 'eco art' opportunities.
- Encourage interpretive and informative site specific art.
- Encourage innovation, public interaction and animation through public art.
- Encourage engagement by the community in experiencing public art - festivals, treasure hunts.
- Important to carefully consider siting of structures so sightlines of CCTV cameras are not blocked.
- Consider and assess maintenance and sustainability issues as part of the design process.



Public art should be woven into the public realm both physically and culturally to enrich the town centre.

safety & security

Creating a safe, welcoming public realm is essential to bring people back into Rotherham Town Centre, both during the day and at night. A number of issues have been raised through the consultation process and on a platform of education, policing and enforcement, quality public realm design can play a significant role in reducing fear of and actual crime. The Secured by Design principles (www.securedbydesign.com) aim to 'design out crime' and have been proven effective in reducing criminal activity.

Safety and security is an over arching theme highlighted throughout the report, however in summary the key considerations include:

- Providing clear sight lines and legibility through signage, street layout and lighting.
- Use of lighting to enliven spaces and highlight key routes.
- Designing out 'negative' spaces and trouble spots.
- Encouraging positive activity and use of the Town Centre, especially after 5pm.
- Ensuring car parks and pedestrian gateways are safe and welcoming environments.
- Planting and public art installations should be designed to maintain open views and prevent the creation of hiding places.
- Encouraging active building use and frontages to provide passive policing.
- Continuing and expanding the existing 'Town Centre Warden' scheme.
- Building civic pride and community ownership of 'public space' and the environment.

public realm specification matrix

The Vision sets the goal of achieving ‘A Harmonious Town Centre’, which is illustrated by design inspiration and a design concept for each of the Standard, High and Premium Quality Palettes (refer to p43).

The following matrix develops this to establish a working specification for the proposed Rotherham Town Centre Public Realm Palette. It proposes styles, finishes and colours to illustrate a ‘design spirit’ to be detailed on a project by project basis. Therefore specific products and manufacturers are given as indicative examples and do not restrict use of equivalent alternatives. This should be considered a baseline for all future schemes.

Standard Quality Palette

surface materials			
Nos	Item	Style	Colours/Finishes/Sizes
1	Pedestrian Paving slabs	1. Concrete paving slabs (Marshalls 'Saxon' natural or equivalent)	1. Natural exposed aggregate in varying size units (Random course pattern)
2	Pedestrian Paving setts	2. Concrete paving setts for accents and kerb band (Marshalls 'Tegula' or equivalent)	2. 'Traditional' colour mix in varying size units (Random course pattern)
3 & 4	Vehicular carriageway	3. Bitmac 4. Concrete paving setts for accents (Marshalls 'Tegula' or equivalent) - for shared surface, key pedestrian crossings and on street parking.	3. As per RMBC typical specification 4. 'Traditional' colour mix in varying size units (Random course pattern)
5	Kerbs	5. Concrete with Yorkstone aggregate Saxon kerbs (Custom designed by Marshalls for Rotherham or equivalent).	5. Natural

lighting				
Nos	Item	Style	Colours/Finishes	Performance Spec.
6	Vehicular scale fixtures	6. Simple elegant post with 'dish' fixture (phase out existing 'globes')	6. Matt black finish, (RAL no 9011 Graphite Black)	6. Minimum level of CE2 (BS-En 13201-2), Colour rendering index of 60 or above. Remote switching methods may be req.
7	Pedestrian scale fixtures	7. Simple elegant post and fixture (site specific and in keeping with the over arching criteria - style, colour, performance)	7. Matt black finish, (RAL no 9011 Graphite Black)	7. Minimum level of CE2 (BS-En 13201-2) Increase to CE1 or CE0 may be required. Colour rendering index of 60 or above. Remote switching methods may be req.

Lighting should be to the Secured by Design standards (www.securedbydesign.com)

street furniture			
Nos	Item	Style	Colours/Finishes
8	Bollards	8. Simple contemporary form	8. Matt black finish with reflective band (RAL no 9011 Graphite Black).
9	Seating	9. Simple contemporary form to include option for arm rests, backs and backless benches	9. Timber slats with steel frame, matt black finish (RAL no 9011 Graphite Black).
10	Tree guards & grilles	10. Simple contemporary form	10. Matt black finish, (RAL no 9011 Graphite Black).
11	Litter bin	11. Simple contemporary form. Square shaped, steel construction bin with sufficient opening to fit 12" pizza box. (Wybone 'City Bin' or equivalent)	11. Matt black finish, (RAL no 9011 Graphite Black).
12	Recycling bins	12. To match style of litter bins. Clear demarkation of various recycling uses.	12. Matt black finish, (RAL no 9011 Graphite Black).
13	Cycle rack	13. Simple contemporary form	13. Matt black finish, (RAL no 9011 Graphite Black)
14	Signage	1. Simple contemporary form	14. Matt black finish with white lettering or white finish with black lettering
15	Railings	15. Rotherham Town Centre ornamental railing to be developed to Highway standard, based on Moorgate Crofts Business Centre.	15. Matt black finish, (RAL no 9011 Graphite Black)
	Other	Other street furniture should be in keeping with the above style and be simple and contemporary in form.	Matt black finish, (RAL no 9011 Graphite Black)

Surface Materials



www.marshalls.co.uk



Lighting



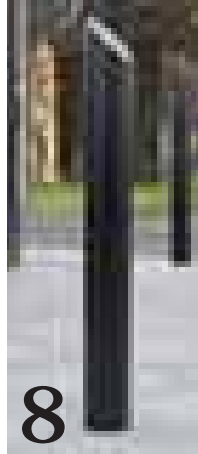
www.aquiladesign.co.uk



Various manufacturers



Street Furniture



www.marshalls.co.uk



www.streetlife.nl



www.woodhouse.co.uk



www.streetlife.nl



www.britannia.uk.com



www.lukehughes.co.uk



www.wybone.co.uk



www.wybone.co.uk



www.marshalls.co.uk



www.woodhouse.co.uk



Campbell Design & Engineering Ltd.

High Quality Palette

surface materials			
Nos	Item	Style	Colours/Finishes
1	Pedestrian Paving slabs	1. Yorkstone Paving Slabs	1. Colour: Deep grey/ochre tones. Finish: Riven/rustic finish. Varying size units (Random course pattern)
2 & 3	Pedestrian Paving setts	2. 'Rotherham Red' sandstone setts - Limited use for accents. 3. Yorkstone setts	2. 'Stoneraise Red' or equivalent, riven finish (Stretcher bond pattern) 3. Grey/buff, riven finish (Stretcher bond pattern)
4 & 5	Vehicular carriageway	4. Bitmac with granite chipping aggregate - for heavy vehicular use. 5. 'Rotherham Mix' Granite setts - for shared surface, key pedestrian crossings and on street parking.	4. Light grey aggregate. Stone mastic asphalt with granite or slag aggregate 5. Mix of Grey and Pink tones to complement 'Rotherham Red' sandstone: <ul style="list-style-type: none">25% Stardust Flamed25% New Rustenburg Bush Hammered G65825% Darton Blend Flamed G354B25% Poppy Flamed G699 - or equivalent Riven finish (Stretcher bond pattern)
6	Kerbs	6. Granite Kerb	6. Mid/light grey (New Rustenburg G658 or equivalent)

lighting

The standard quality palette should be applied as a baseline, but high quality areas should encourage increased investment to introduce creative lighting elements

Nos	Item	Style	Colours/Finishes	Performance Spec
7	Vehicular scale fixtures	6. Simple elegant post with 'dish' fixture (phase out existing 'globes')	7. Matt black finish, (RAL no 9011 Graphite Black)	7. Minimum level of CE2 (BS-En 13201-2), Colour rendering index of 60 or above. Remote switching methods may be req.
8	Pedestrian scale fixtures	7. Simple elegant post and fixture (site specific and in keeping with the over arching criteria - style, colour, performance)	8. Matt black finish, (RAL no 9011 Graphite Black)	8. Minimum level of CE2 (BS-En 13201-2) Increase to CE1 or CE0 may be required. Colour rendering index of 60 or above. Remote switching methods may be req.
9	Creative Lighting	9. In ground uplights, building mounted, bollards, light installations etc.	9. Matt black finish, (RAL no 9011 Graphite Black)	

Lighting should be to the Secured by Design standards (www.securedbydesign.com)

street furniture

The standard quality palette should be applied as a baseline, but high quality areas should encourage increased investment and site specific detailing. Where appropriate, variation to the standard quality palette should be high quality 'off the shelf' elements that are coherent with the standard palette in style, colour and performance.

Nos	Item	Supplier: Name/Model	Colours/Finishes
10	Street furniture	10. Simple contemporary form	10. Matt black finish, (RAL no 9011 Graphite Black)

Surface Materials Pedestrian Areas

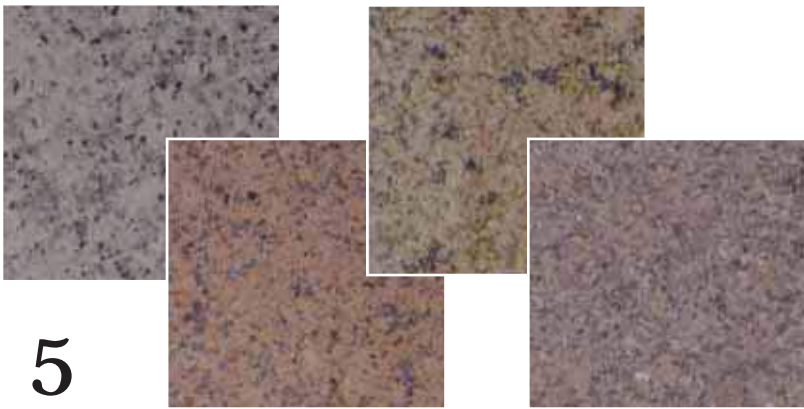


www.marshalls.co.uk



www.hardscape.co.uk

Shared Surface - Carriageway (Rotherham mix)



www.hardscape.co.uk

Kerb



Lighting



www.aquiladesign.co.uk

7/8



8



Various manufacturers



9

Street Furniture



www.wybone.co.uk



www.marshalls.co.uk



www.woodhouse.co.uk



Campbell Design & Engineering Ltd.

Street Furniture



10

www.streetlife.nl



www.woodhouse.co.uk



www.streetlife.nl



www.britannia.uk.com



www.lukehughes.co.uk

Premium Quality Palette

surface materials

The high quality palette approach for surface materials should be applied to all premium quality areas to unify streets and spaces within the town. Where appropriate new surface materials such as recycled glass can be introduced when good reason can be demonstrated.

lighting

A bespoke and innovative design approach to statutory and creative lighting should be applied to all premium quality areas. This can either complement the high and standard quality palettes or instead contrast when good reason can be demonstrated. Creativity should be encouraged in premium quality areas.

Lighting should be to the Secured by Design standards (www.securedbydesign.com)

street furniture

A bespoke and innovative design approach to furniture should be applied to all premium quality areas. This can either complement the high and standard quality palettes or instead contrast when good reason can be demonstrated. Creativity should be encouraged in premium quality areas.

Bespoke Furniture Examples



Bespoke stainless steel seating and bin situated in Gateshead, designed by John Creed.



Supplier: Broadbent. Bespoke bench in Millennium Square.



Bespoke seating in Newcastle.



Bespoke furniture in Millennium Newcastle.

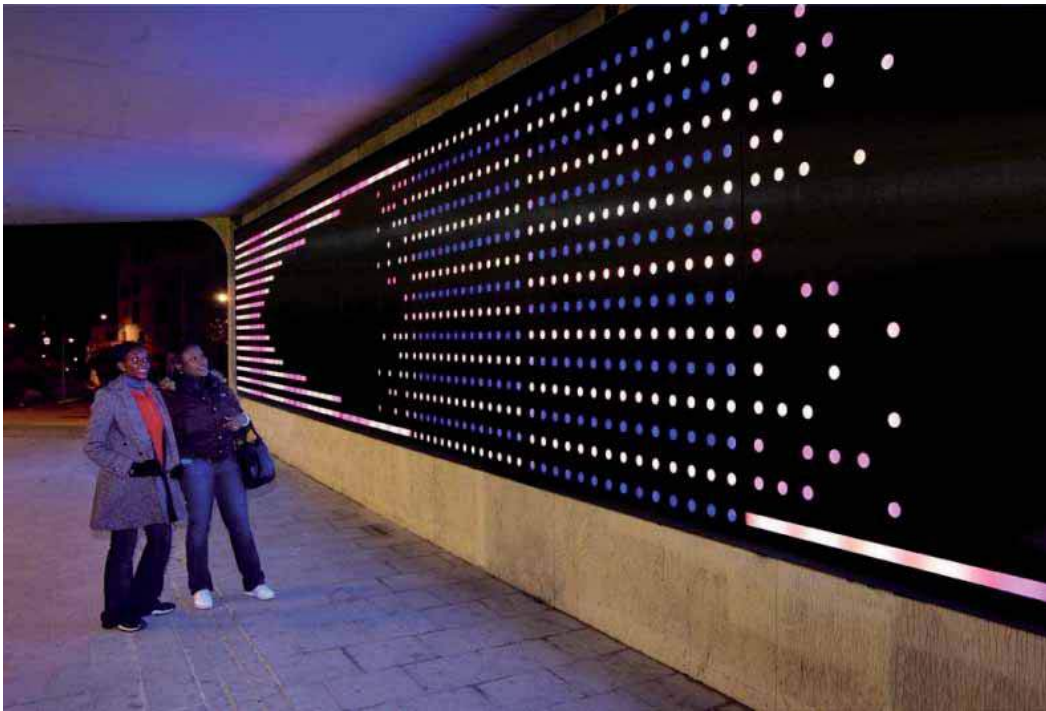


Bespoke seating in Darlington Centre.

Bespoke Lighting Examples



Bespoke lighting 'Lightwaves' by Raphael Daden' in Tanner Street, London Borough of Barking & Dagenham. Supplier: Aquila Design.



Bespoke lighting 'Morrison Bridge'. Supplier: Aquila Design.



Victoria and Albert Museum and Garden. Supplier: Guzzini.



delivery, management & maintenance

- Delivery
- Management & Maintenance

section 7

delivery

Strategic Fit and Conflicts Check List

The following chart sets out a process to guide and review a new project against the aspirations of the Public Realm Strategy to identify:

- Strategic Fit and Conflicts.
- Potential Solutions to Strengthen and Align Proposals.

New Project



Step 1 - Identify the Vision

- ☐ Does the new project broadly support the Public Realm Strategy Objectives (Refer to p6).
- ☐ Does the new project advance/safeguard the Public Realm Strategy's Four Vision statements (refer to p41).



Step 2 - Identify the appropriate Public Realm Treatment

- ☐ Which Zone of Influence is the new project located in (refer to p80).
 - Consider the 'design tone' described in the Zone of Influence and the new project's fit.
 - Consider how the new project can advance/safeguard the intent shown by the Public Realm Strategy Propositions.
- ☐ Does the new project demonstrate the principles shown in the Public Realm Strategy Design Guidelines (Refer to p107).
 - Consider layout and design of public realm elements.
 - Consider Style, Material and Performance of Surface Materials, Lighting and Street Furniture.
- ☐ Where does the new project fit within the Spatial Hierarchy and what is the appropriate level of public realm investment (Standard, High or Premium quality palette - refer to p79).
 - Consider the relevant Public Realm Palette Design Concepts (refer to p43) in the development of the public realm.
 - Consider the site's evolving context in the development of the public realm.

Step 3 - Identify the Delivery, Management and Maintenance Mechanisms

- ☐ Does the new project demonstrate the principles outlined in the Public Realm Strategy Delivery & Maintenance Section (refer to p139).



Step 4 - Produce a Strategic Fit and Conflicts Report

- ☐ Set out recommendations to strengthen and align the new project with the aspirations of the Public realm Strategy as part of an iterative process.



Step 5 - Continue to Develop the Public Realm Strategy

- ☐ What are the implications of the new project on the Public Realm Strategy? The document is intended to deliver a flexible tool-kit approach for Rotherham Town Centre and should continuously be re-visited as the Town Centre evolves.

management & maintenance

Rotherham Town Centre is on the cusp of major change and in preparation for the challenges ahead requires a step change in approach and funding for management and maintenance. Effective and creative management and maintenance of streets and spaces is the key to sustainable and successful public realm.

An Integral part of the Design Process

At the outset, high quality design and implementation in public realm improvements provides a strong platform for successful places, however with it brings responsibility in the long term. To be truly effective, the role of management and maintenance needs to begin during the design process of each individual project. The long term upkeep of a scheme should be considered from the very beginning so informed choices can be made on issues such as materials, street furniture, lighting and the layout and function of streets and spaces. This should not lead to 'maintenance led' design but instead ensure the important considerations and implications associated with it are integrally considered in the development of proposals. In addition, those responsible for management and maintenance of public realm are often required to take on newly completed schemes that they have had no involvement in. A joined-up approach from the outset of any project in terms of considering management and maintenance with all key stakeholders involved can alleviate financial pressure and gain consensus on the best approach. This can ensure provision of sufficient

resources and monies to be available at completion. Future maintenance arrangements including funding should be laid out at the time the Capital scheme is approved. Each project should result in a maintenance plan that can both feed into and be guided by RMBC strategic thinking. An individual project's maintenance plan should:

- incorporate the agreed procedures for maintenance works.
- identify exact materials to be used.
- provide the names and contact details for all suppliers.
- outline procedures for reinstatement works by public utilities companies.
- set out a cleaning regime.
- set out maintenance regime.
- set out an inspection regime and response times for cleaning and repair works.
- set out a chewing gum removal regime.

A Comprehensive Strategy

On a strategic level it is recommended that a Rotherham Town Centre Management and Maintenance Manual/Charter is created. This would include general standards, guidance, responsibilities and monitoring procedures specifically for the Town Centre and in addition technical requirements of individual projects. This can provide a cross departmental tool-kit for RMBC and tailored guidance for private developers and designers. This concept has been successful for a number of local authorities including Glasgow City Council and Sheffield City Council. As part of this, Sheffield published a comprehensive overview on their website, including a hierarchy plan that illustrates zones of the City Centre and their relative priority in terms of maintenance investment (Platinum, Gold, Silver and Bronze zones). This allows resources to be efficiently focused to key areas and reinforces the notion that one size cannot fit all in our urban centres. RMBC currently address street cleansing through focusing resources on busier areas, which can be expanded upon and formalised as part of a wider programme.

The Rotherham Town Centre management and Maintenance Manual/Charter should set out:

- What are the roles and remit of individuals and various departments - can they be brought under a single umbrella?
- What maintenance/street cleansing is carried out, what is the regime and how does it reflect the hierarchy of streets and spaces.
- Set streamlined processes for swift reporting of problems and carrying out of work (which RMBC have successfully introduced).
- What maintenance is required for individual schemes in man hours and cost.
- A Town Centre wide breakdown of costs for each street and space that can be updated to accommodate new schemes.
- Total costs of annual maintenance/street cleaning expenditures and production of forecasts taking on board future aspirations and the implementation of new schemes (to understand and plan for implications of public realm projects from the outset).

To provide a safe, welcoming attractive and clean environment for Rotherham Town Centre, a clear strategy for the following elements is required:

1. Town Centre Wardens

Rotherham has implemented a Town Centre Warden scheme currently with two wardens providing a valuable caretaker role, focusing on safety and street cleansing issues. In other towns and cities this front line approach has been expanded to promote the ‘welcome’ message such as Sheffield’s Ambassadors. The Ambassadors have a wide remit including, meeting and greeting, first aid, enforcement and education. This approach should be considered in Rotherham to pro-actively build community trust and enthusiasm as redevelopment transforms their Town Centre over the following years. Funding and continuity of such schemes can be challenging, however innovative approaches such as tying in with ‘Intermediate Labour Market Initiatives’ have been successful elsewhere.

2. Street Cleansing

Rotherham Town Centre along with many other towns and cities has a problem with litter, particularly in high use areas such as retail streets and areas around bars and clubs. This creates a negative impression and can result in decreased respect for the Town Centre.

The first line of defence is education and awareness campaigns throughout all parts of the community including schools, local press and public events, reinforcing the importance and community value of public realm. RMBC have carried out various awareness schemes to date and continued funding is essential as the town embarks on the major change planned for the Town Centre. The design process of new improvements should be inclusive to create community ownership of their Town Centre and the resulting schemes should aim to ‘design out’ litter through well located convenient facilities and appropriate surface materials that can withstand general wear and tear, chewing gum and trouble spots (such as bar/club spill out, smoking areas, vehicular overrun).

A street cleansing regime tailored to the varying needs of areas within the Town Centre along with a recognisable enforcement presence is recommended.

The street cleansing regime for Rotherham Town Centre should include:

- A clear hierarchy of treatment and intensity of cleansing for areas within the Town Centre.
- Targets to measure success (ideally publicly advertised to build trust).
- Consider flexible regimes to cater for the evolving Town Centre and peak times, such as intensifying street cleaning at certain times (evening economy, Christmas shopping, festivals and events). Different working hours and the potential to operate 7 days a week can help support this.
- Continued investment in suitable equipment to make street cleansing more efficient.

3. Public Realm Aftercare

Rotherham, like many towns has suffered from poor reinstatement works following public utility and public authority instigated work. Failure to reinstate street surfaces to the standard of the original finish, and in a consistent way, will compromise the character, quality and in some cases functionality of the streetscape. The New Roads and Streetworks Act 1991 seeks to ensure that surfaces are quickly and effectively restored to the original standard as soon as the necessary works have been carried out. Repairs to road and pavement surfaces are to be done with ‘prescribed materials’ to a ‘prescribed standard’ and streetworks supervisors and operators have to prove themselves capable of organising and carrying out reinstatement works. The Act is a positive instrument for achieving a higher standard of quality control and should be enforced vigorously to ensure that a quality public realm is not undermined.

Material failures, or damage (through accident or vandalism), must be dealt with immediately to ensure safety of users and preserve the quality of the environment. When problems occur, swift reinstatement sends a positive message to the public and therefore processes for reporting of and carrying out works must be in place and clear in terms of responsibilities and expectations. To be able to carry this out quickly, efficiently and affordably, it is important to have a reliable and accessible supply of replacement materials. During the design and implementation of any public realm project, it is important that this issue is carefully considered for all surface materials and street furniture. While specification should not be wholly dictated by lead in time, the majority of materials and elements through the Town Centre should be quickly and easily accessible. Limited use of specialist and more challenging materials and elements has a valuable place in the public realm and good planning and contingencies should be in place to accommodate this. In addition to selecting materials and elements with minimal lead in times and from sustainable sources, other methods such as stockpiling (in-house or by suppliers) or agreements with suppliers can be used. RMBC have established relationships with suppliers, however there is a requirement for appropriate and secure storage space, should in-house stockpiling be pursued further.

Funding Opportunities:

The ambitious scale of change proposed for Rotherham Town Centre brings both new opportunities and responsibility to protect investment in the long term for residents and visitors to enjoy. This inevitably puts pressure on already tight maintenance budgets leading to a greater need to focus precious resources efficiently and prioritise prudently. Regeneration will bring investment, development and intensification of use in Rotherham Town Centre resulting in new opportunities to raise funds. The competition for additional money will be intense, therefore the management and maintenance agenda must present a unified and comprehensive bid to safeguard or ‘ring fence’ the additional funding required. This reiterates the importance of both a strategic Management and Maintenance Charter/Manual and the bottom up approach to embedded involvement in new projects to feed into the strategy.

A number of solutions have been utilised by local authorities to raise funds:

Commuted Sums:

- 1. Ring fencing funds from a new development to finance an agreed management/maintenance plan for the associated public realm. This approach has successfully been achieved in Rotherham through the Westgate Demonstrator Project.
- 2. Collecting funds into an overarching Town Centre wide pot from a variety of development contributions.

European Funding:

A comprehensive approach to management and maintenance both strategically and for individual projects can provide local authorities a tool for attracting funding as various streams develop. Glasgow City Council were, for example able to secure European funding to support some of the upkeep of their ambitious programme of public realm improvements.

Seeking Endowments from Capital Funding:

As part of capital improvements a portion of the budget is set aside for management and maintenance. This fund can be invested and managed by either a local authority or management company. This allows individual schemes varying and evolving needs to be considered and accommodated. In many cases the management company model allows stakeholders and funders continued involvement in a scheme.

Private Ownership and Management:

The renaissance of our urban retail centres has brought large scale developer led schemes into town and city centres. Local authorities are increasingly pursuing agreements by which developers retain ownership and responsibility for the management and maintenance of publicly accessible streets and spaces. The benefits are that the developer is able to safeguard their investment and local authorities can achieve a step change without the potential financial burden of adopting schemes. However this approach has been much debated in terms of its effect of authenticity of ‘public space’ and the potential loss of the character and soul that urban centres evoke. There is also a risk that ‘over management’ leads to a loss of personal responsibility and awareness.

